

03

Urban Design Strategy

Setting, context, and heritage

- respect the surrounding urban grain, land use, building types and scale
- celebrate the historic and distinctive architectural features of the area

Liveable, lifetime neighbourhoods

- a flexible block plan and street grid
- a mix of dwelling types and sizes, including family housing
- high quality shared and/or private amenity space for all residents
- mansion blocks and some taller buildings (for private residential)
- mixed-use development

Permeability and connectivity

- active street frontages
- attractive routes to encourage walking and cycling
- new east-west connections into RBKC
- improved links over the Central Line cutting
- new north-south routes connecting Westfield to Imperial College
- new connections under the Hammersmith & City Line viaduct and the A40
- new connections between Westfield and Hammersmith Park through the BBC TV Centre site
- improved connections throughout the town centre and with its hinterland
- enhance links to Wormwood Scrubs

Public realm

- White City Green, a major new public open space
- manageable, inclusive, safe and useable public realm
- make use of the space under the Westway/A40
- enhance views of BBC TV Centre and make best use of the forecourt
- maintain open space around the Hammersmith & City Line viaduct
- reclaim the area to the north of Westfield from the tangle of roads and servicing facilities

Strategic context

3.1 The urban design strategy for White City will help deliver wider aspirations to connect to the Opportunity Areas at Old Oak and Earls Court. This will deliver improved pedestrian and cycle links between the areas by enabling better access to existing park spaces and requiring the creation of new park spaces to support the housing development coming forward. This in turn should reduce pressure on public transport and car use and contribute to a greener more sustainable city.

3.2 The quality of environment drives investment decisions both nationally and internationally and it is important that a high benchmark is set within White City and its neighbouring OAs to ensure that both economic and environmental benefits are maximised.

3.3 In the longer term it would be possible to walk or cycle through public open spaces from Willesden Junction to Shepherds Bush and Earls Court, and for residents of Royal Borough of

Kensington and Chelsea and Hammersmith and Fulham to make use of amenities and social infrastructure in their respective boroughs easily and conveniently without having to detour around major pieces of transport infrastructure.

3.4 This overall strategy is illustrated in figure 3.2. The detailed masterplan that has been developed for White City sets out how this broader aspiration will be delivered.

3.5 White City forms one piece of a larger city planning jigsaw, with careful design it can contribute to not only enhancing its immediate environs but also make a significant contribution to the regeneration of west London, enhancing London's world city status.

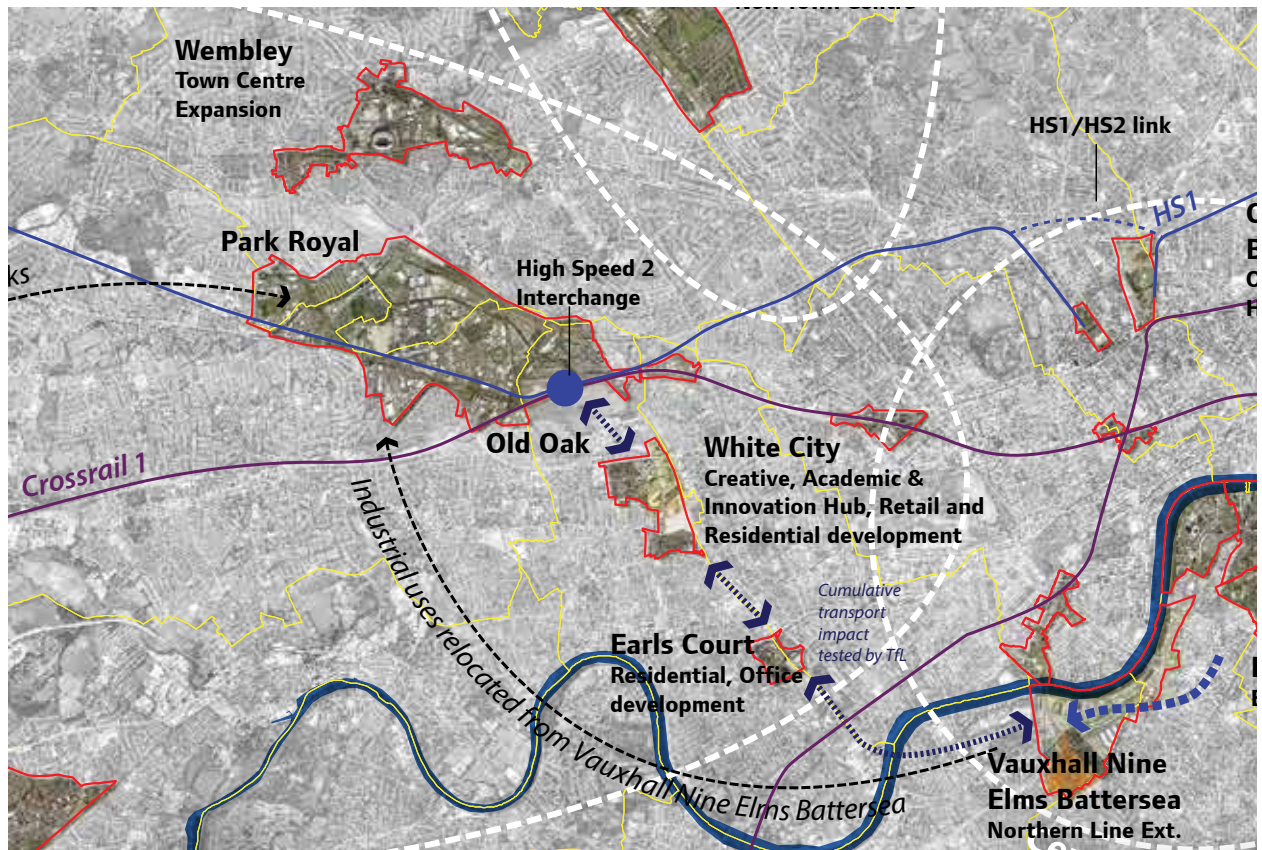


Figure 3.1: West London context

- Existing green open space
- Proposed open space
- OA boundary
- Strategic links/spaces
- Development opportunity



Figure 3.2 Urban design strategy, New green spaces and links

Historic context

Early development

3.6 Counters Creek originally ran through the Opportunity Area along the alignment of the West Cross Route, continuing on to Lots Road where it met the Thames. The area was largely farmland until the mid-1800s when railways were built over the land to lead into the city, with the West London Line in 1844, the Hammersmith & City Line in 1864 and the depot and generating station for the Central Line in 1900.

3.7 As a result of these new transport links, by the turn of the century the area around Shepherd's Bush Common had become a shopping and entertainment centre for West London with new shopping parades, cinemas and theatres. Semi-detached villas and terraced housing of this growing and prosperous suburb lined the streets beyond the Common, leaving a legacy of relatively dense Victorian housing which remains the dominant residential character.

The first Olympic legacy

3.8 In 1908 the first of several international exhibitions, the Franco-British Exhibition, was held on grounds to the west of Wood Lane, in buildings rendered in white painted stucco, from which the site got its name "White City". Two new underground stations at Wood Lane (Central and Hammersmith & City Line) catered for the millions of visitors to the Exhibition.

3.9 The exhibition grounds featured a network of roads, bridges, waterways, as well as a stadium which was used for the 1908 Olympic Games. Although the exhibition buildings were designed as temporary structures, exhibitions continued to be held there until the late 1930's. The stadium was used by Queen's Park Rangers Football Club until they moved to Loftus Road, and continued to be used for rugby, athletics, boxing and greyhound racing until the mid-1980s.

3.10 Today, the diagonal road alignment between the White City estate, the BBC Media village and the grounds of Hammersmith Park are the only physical characteristics left of this early history of the site.



① 1908 - White City Olympic Stadium



② 1908 - White City Exhibition



③ White City Exhibition: The Japanese garden, now in Hammersmith park



④ Shepherd's Bush Empire

1: Olympic stadium (1908)

2: Franco-British Exhibition buildings (1908)

3: Japanese garden

4: Shepherd's Bush Entertainment Venues

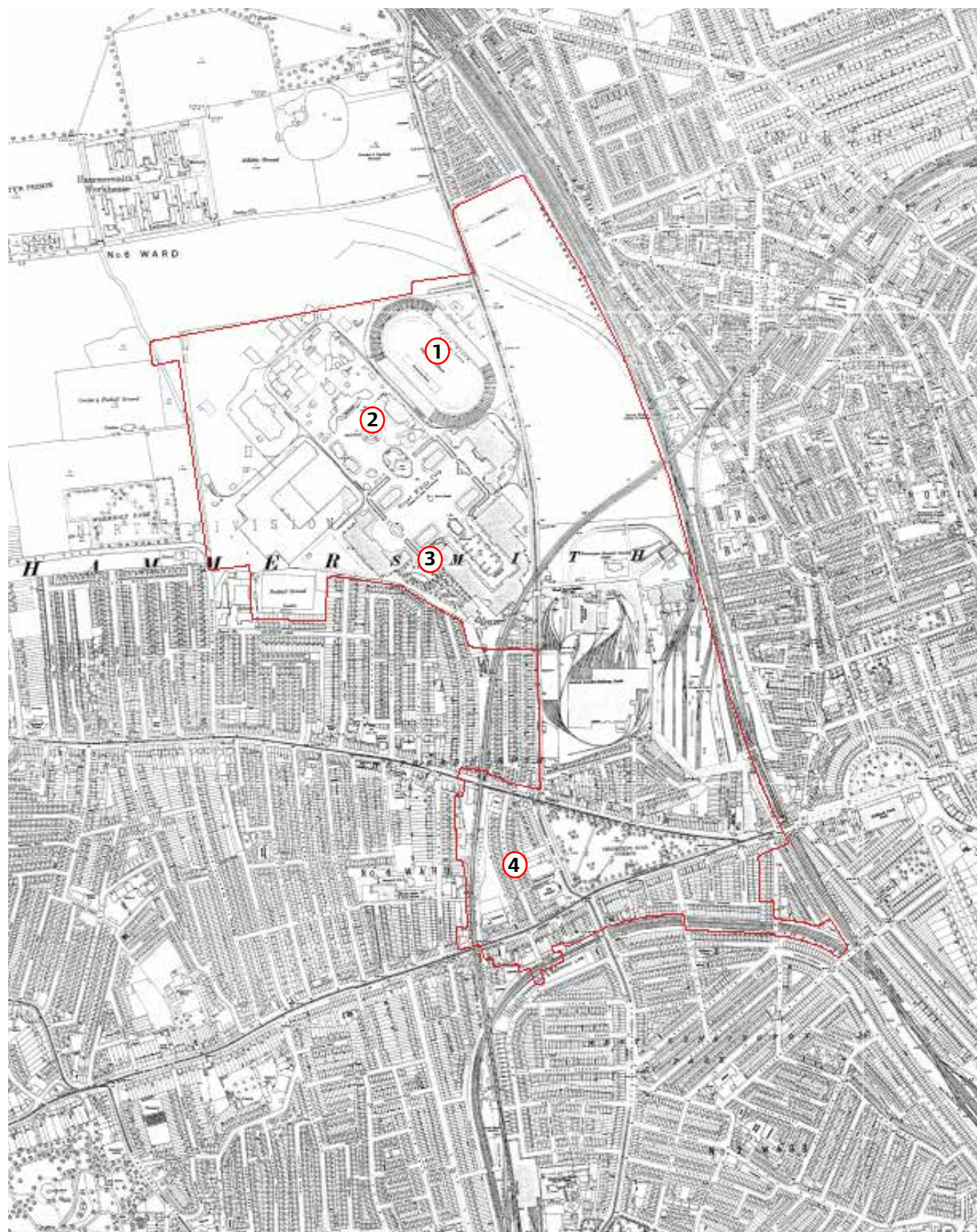


Figure 3.3 White City Opportunity Area: 1916

(Source: LB H&F)

Inter and post war development

3.11 In the late 1930s the London County Council built the White City Estate on the Western part of the White City exhibition grounds; more than 2,000 homes in 5 and 6 storey blocks in a grid plan, with tree-lined streets named after exhibition buildings, such as Australia Way and Commonwealth Avenue. At the time the housing was of an exemplary standard although the rigid grid approach contrasted to the garden suburb approach taken to the west of the estate and the north of the Westway / A40.

3.12 After WWII, the BBC Television Centre and Hammersmith Park were built on the southern part of the exhibition grounds. Graham Dawbarn, of the firm Norman and Dawbarn, designed the TV Centre, which was Britain's first purpose built TV centre.

3.13 A new Central Line underground station was built at White City in 1947 to replace the earlier Wood Lane station. Between the Central Line cutting and the West London Line, industrial uses developed in the sites between the new road and rail infrastructure.

3.14 During the late 1960s the elevated Westway motorway was built to link the Marylebone Road with Western Avenue. Other major transport infrastructure developments included the West Cross Route (now the A3220) which linked the elevated gyratory at the Westway with a new roundabout at Holland Park. This was part of the inner London motorway box proposed in the 1969 Greater London Development Plan that was never completed following rigorous protests from community groups.

3.15 The BBC expanded Television Centre and while the White City stadium continued to be used for various sporting events, including the 1966 World Cup, it was demolished in 1985 to make way for the BBC Media Village designed by Allies & Morrison. The opening of the Westfield development in 2008 created a huge retail and leisure destination with improved transport facilities including a new Underground station at Wood Lane and a new Overground train station on the West London Line.



① Construction of the Westway (1960s)

Source: LB H&F archives



② White City Estate

Source: LB H&F archives



③ White City LUL Station, Central Line

Source: TfL



④ BBC TV Centre under construction

Source: BBC

- 1: Westway /A40 (1964)
- 2: White City Estate (1936)
- 3: White City LUL station (1947)

- 4: BBC TV Centre (1960)
- 5: Shepherd's Bush Common
- 6: West Cross Route (1960)

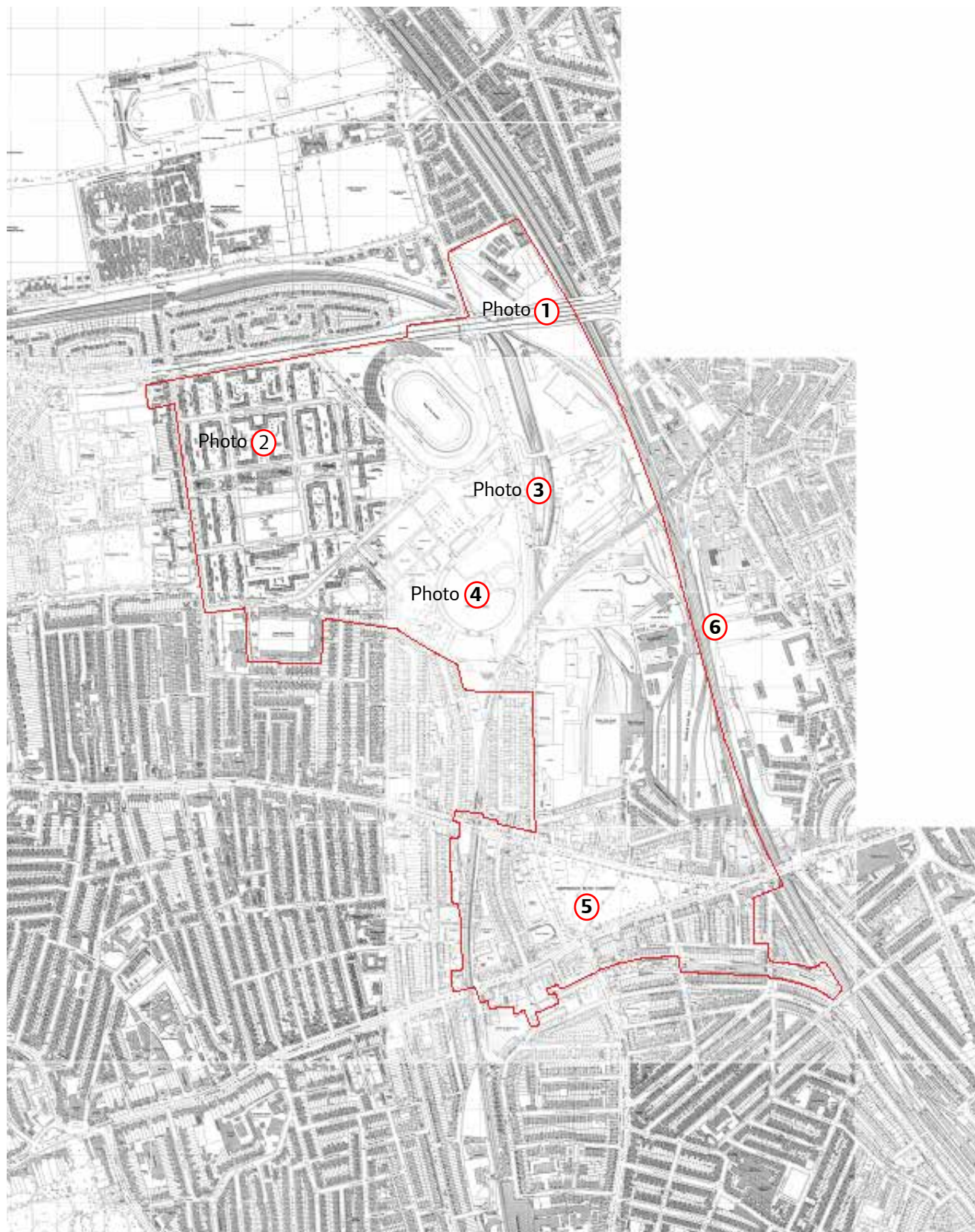


Figure 3.4 White City opportunity Area: 1978

(Source: LB H&F)

Heritage Assets

Conservation Areas

3.16 Most of White City East is located within the Wood Lane Conservation Area. The Shepherds Bush Conservation Area is located in the OA but is centred at the south of the area around the town centre and green and surrounding residential areas. Beyond the Opportunity Area boundary are several Conservation Areas that are predominantly residential, including the nearby Old Oak and Wormholt Conservation Area and the Oxford Gardens Conservation Area in RBKC.

Wood Lane Conservation Area

3.17 White City East is largely covered by this conservation area which comprises distinctly different building types and areas of character. The area is centred around BBC TV Centre with other key features such as the White City London Underground Station, the Wood Lane Estate and Hammersmith Park.

3.18 The Grade II Listed BBC TV Centre is the most prominent of listed buildings in the area. The celebrated BBC Television Centre and Studio 1 has historic interest as the country's first purpose-built studio television complex. Its distinctive circular drum and contemporary detailing exemplified on the facade of Studio 1 are key elements of its special architectural and historic interest.

3.19 The DIMCO Building is Grade II Listed, which is a former electricity generating station built in 1898 as the engine and boiler house to serve the Central Line. It is now the earliest example of an electricity generating station built for the London Underground Railway system. The generator closed in 1928 and the building was later used by the Dimco power tool company.

3.20 The White City London Underground Station is designated a Building of Merit by the Council.

Shepherd's Bush Conservation Area

3.21 This conservation area in the south of the OA includes the whole of Shepherd's Bush Common and surrounding residential streets.

Shepherd's Bush Common forms the hub of the area. It is an important open space for residents and visitors to the area and provides the focus for the area. It provides the setting for the built form, which includes some fine buildings most notably the listed grade II Empire Theatre and the listed grade II former Odeon Cinema. The neighbouring buildings at no.54 and no.58 Shepherd's Bush Green are both included on the Councils list of Buildings of Merit. The CA also includes the grade II listed terraces directly south of the Westfield mall, behind the main frontages on Uxbridge Road. The Grampians (listed grade II) is also of a high architectural quality and makes a positive contribution to the area.



A Shepherd's Bush green

Source: LB H&F



B BBC TV centre

Source: GLA



C DIMCO building

Source: GLA

- | | |
|--|--|
| <ul style="list-style-type: none"> Listed Building Buildings of merit Conservation Area | <p>Conservation Areas:</p> <ul style="list-style-type: none"> 1: Oxford Gardens 2: Old Oak and Wormholt 3: Wood Lane 4: Avondale Park Gardens 5: Ingersoll and Armingier 6: Norland 7: Shepherd's Bush 8: Holland Park 9: Coningham and Lime Grove 10: Hammersmith Grove 11: Melrose 12: Lakeside / Sinclair / Blythe Road |
|--|--|



Figure 3.5: Conservation Areas & listed buildings

Urban form

3.22 White City today boasts iconic buildings, a strong heritage of creative industry and leisure attractions, valued open spaces and landscapes, and internationally renowned institutions. It also contains many buildings of poor quality including industrial sheds and dated commercial space.

3.23 Wood Lane today is the key transport corridor which acts as a principal gateway to the area. Many of the key commercial uses of the OA are focused along this road, with the predominant scale of development reflecting industrial and commercial uses. The grade II listed BBC TV centre and the new BBC Media Village are the dominant buildings along this part of Wood Lane. The low rise Wood Lane Estate separates the two. Low rise office developments of 6-storeys and two tube stations are located on the east side of Wood Lane. Large warehouses and industrial sheds sit on large plots of land behind many of which contain buildings of poor quality or are dilapidated. Further south on the east side of the road is the Westfield London Shopping Centre which sits opposite a long row of 3 storey Victorian terraced housing. A 6-storey car park owned by the BBC is south of the Wood Lane railway bridge. The 15-storey east tower was a later addition to the BBC TV Centre is set back from Wood Lane and is prominent in longer distance views.

Town Centre

3.24 The town centre has great variation in building form and styles. Five-storey Victorian terrace buildings predominate along the north side of the green, which is set beside the large modern Shepherds Bush Underground Station at its eastern end. Offices of 5-6 storeys predominate the western side of the green, along with larger buildings for entertainment such as the Shepherds Bush Empire and Shepherds Bush Pavilion. The southern side provides a mix of Victorian and Art Deco buildings set adjacent to the 20-storey Charecroft Estate which sits above the W12 Shopping Centre which is 2 storeys at its frontage.

Existing Housing in the area

3.25 The area surrounding White City includes different forms of housing, charting the history of housing development in London with pre- and post-war estates at high and low density, Victorian terraces and mansion blocks. Immediately north of the OA are a number of streets with consistent 2 storey terrace dwellings. To the north-east are North Kensington and the Oxford Gardens Conservation Area which feature good quality terraced housing, while to the east and south of the Westway, the Stable Way gypsy and traveller site sits in a pocket of land between the roads of the Westway junction. Beyond the OA boundary to the east are several clusters of residential towers in the Lancaster West, Silchester and Edward Woods estates which comprise 21-24 storeys. To the south and south west of the opportunity area, the area comprises a mix of housing around the town centre, predominantly consisting of Victorian terraced housing.

3.26 Council estates make up a significant part of the housing stock within the Opportunity Area. The Wood Lane estate is a tightly packed development of two to four storey townhouses with integral porches, terraces and balconies, ramps and stairs, with an emphasis on private entrances in a generally pedestrianised environment. The White City Estate is characterised by larger 5/6 storey blocks of flats set to a regular perpendicular plan with external corridor access. Mature trees line the grid of streets in the estate with generous open spaces provided between the buildings. The estate also features several community facilities and local shops, churches and schools, which all form a strong central focal point for residents. The nearby Batman Close Estate features buildings of 3-5 storeys around a large area of central open space.

- >10 m
- 10 - 20 m
- >20 m

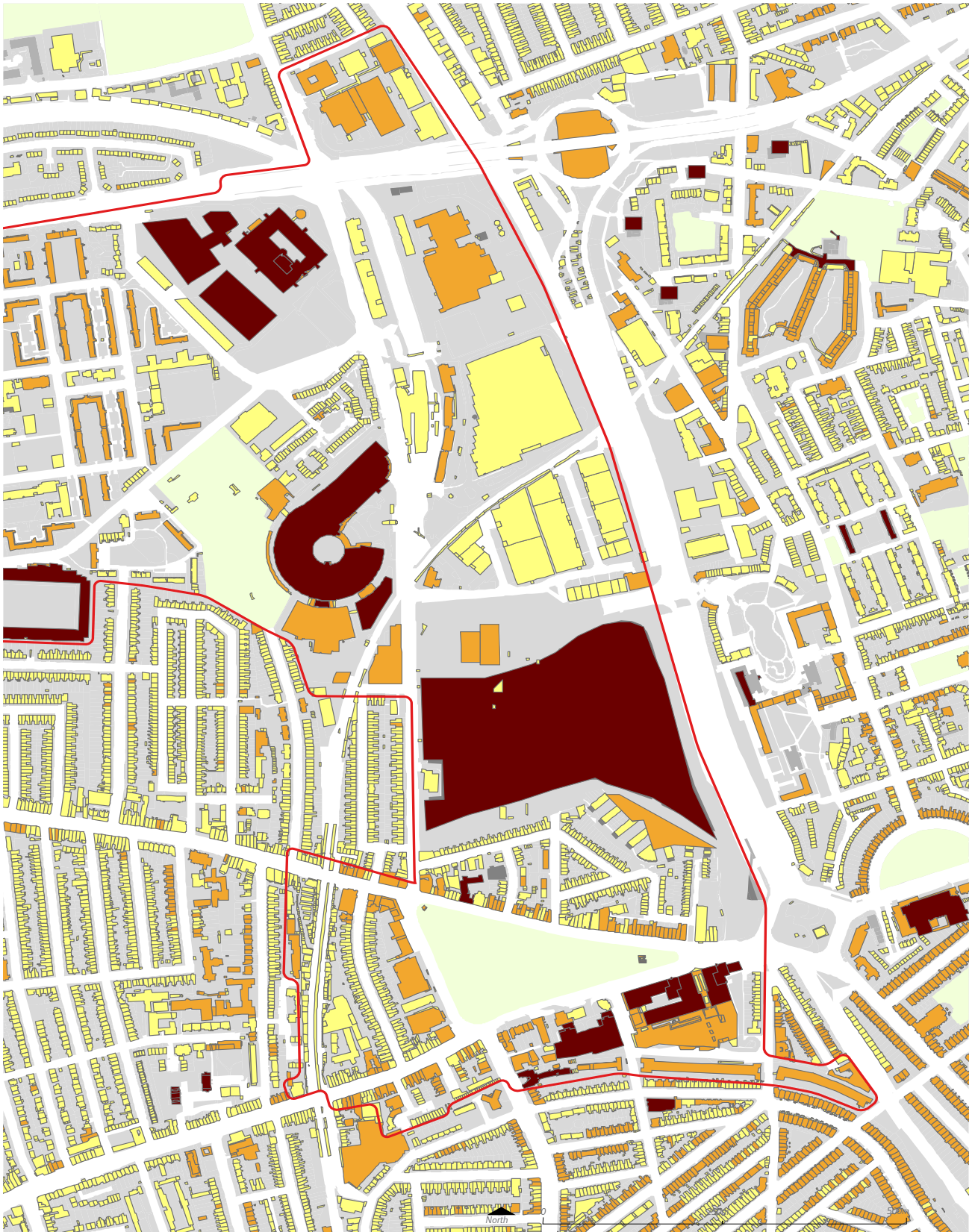


Figure 3.6: Existing building heights

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Opportunities

3.27 This framework seeks to integrate new development with existing facilities for community, sport and creative industries, provide better access to existing and new facilities and open spaces, and identify what new further facilities would be required to support local growth and what contributions developers would make to that provision.

Open Space

3.28 The OA boasts a variety of open spaces, including Hammersmith Park, play areas within the estates, informal open spaces such as the lawn in front of the BBC media village and Wormwood Scrubs. These local parks and amenity spaces are popular and well-used. However, most of White City East is within an area of open space deficiency which will need to be rectified as development comes forward by providing new open space and better connections to existing open space as part of the London Green Grid.

Sport and leisure

3.29 The legacy of the 1908 Olympic Games is continued in the area with sporting facilities including the Linford Christie stadium, the Phoenix Swimming Pool, the football pitches at Hammersmith Park, the Westway Stables and the Westway Sports and Climbing Centre.

Retail and town centre attractions

3.30 Retail has always been a key element of the town centre, with Shepherd's Bush Market and the West 12 Centre as local shopping destinations. The arrival of the Westfield centre in 2008 has vastly expanded the retail presence in the area, as well as the leisure offer with many new restaurants and a cinema to complement the existing leisure uses in the town centre.

Culture and entertainment

3.31 Shepherd's Bush has long been a centre for entertainment and leisure, with established venues such as the Empire and the Bush Theatre along with smaller bars and clubs centred around Shepherd's Bush Common. Access / links from the north to the town centre could be improved.



① Linford Christie stadium

Source: Google Maps



② Westfield London

Source: Westfield



③ Shepherd's Bush Empire

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④ The West London Community Riding Centre













- | | | |
|--|--|---|
|  parks |  swimming |  running |
|  playing fields |  cycling |  equestrian facilities |
|  shopping |  soccer |  tennis |
|  entertainment |  basketball |  climbing |



Figure 3.7: Existing Cultural, sport and leisure facilities in White City

Challenges

3.32 The Opportunity Area is highly accessible by roads and rail, but is at the same time blighted by barriers to movement caused by transport infrastructure, with many of the development sites suffering from poor connections to the wider area. The West London Line, access roads and ramps for the West Cross Route and the elevated A40/Westway, the Central Line cutting and the Hammersmith & City Line viaduct all represent physical barriers to key sites in the area and linkages to communities beyond the OA.

3.33 The area is also isolated from its wider context by infrastructure and large building sites which have restricted movement throughout or around them with fencing or inactive frontages, such as the BBC TV Centre and north of the existing Westfield London shopping centre, which have previously discouraged connectivity and permeability through their sites to the wider area.

3.34 Despite the great connectivity of the area in terms of public transport and its proximity to major roads, the legacy of industrial uses to the east of Wood Lane has left a gap in access and there is currently no provision for cycling and pedestrian connectivity throughout White City east.

3.35 There are many changes in levels across the opportunity area hence there is a challenge to achieve acceptable step free routes that are accessible to disabled pedestrians to mitigate changes in level and gradients throughout the OA.

3.36 Connectivity to and from the White City Estates should be improved

3.37 This framework addresses these challenges through a series of urban design objectives that will deliver a new high quality connected urban environment.



① Under the Westway



② Central Line cutting

Source: GLA



③ West Cross Route / A3220

Source: GLA



④ Westfield London

Source: GLA



⑤ BBC Media Village

Source: GLA



Figure 3.8: Challenges in the White City Opportunity Area

Objectives

3.38 There are three principal design objectives for the OA, as illustrated in the Indicative Masterplan:

1. Creating areas of new public realm and open space

- Seeking provision of a high quality area of public open space, preferably on each side of the Hammersmith and City Line viaduct, to give the area identity and provide a recreational space for shoppers, residents and workers.
- Providing smaller areas of open space which will enhance local character and distinctiveness and be clearly defined as private or shared amenity space.
- Integrating existing parks, open spaces and landscaping with new public realm.
- Providing play areas which reflect diverse needs.
- Provide ecological corridors to encourage rich biodiversity throughout the area.

2. Maximising connectivity

- Providing new linkages to overcome barriers created by the Westway, A3220, West London Line, the Central Line cutting and Hammersmith and City Line viaduct.
- Ensuring new developments should be permeable and existing buildings with large impermeable building footprints should provide new links through their sites to provide better connections to surrounding areas.

- Design new streets with active frontages and to be tree-lined with identified routes that will improve conditions for pedestrians and cyclists while reducing congestion.

3. Quality urban design that responds to context

- Provide the majority of buildings at a height of 6-10 storeys in the area to allow for a mix of uses, while also providing some lower rise terraces adjacent to existing neighbourhoods.
- Taller buildings would be more appropriate along the elevated Westway, where they would act as a point of identification for White City.
- Build upon the character of the surrounding high quality areas in West London and provide mansion-block typologies for residential areas.
- Improve settings for the distinctive architectural and townscape features of the area including DIMCO, BBC TV Centre, the Westway and the Hammersmith and City line viaduct.
- New retail should focus on shops within a shopping street typology.
- Provide a transition in scale of buildings from the town centre, moving north through White City East.
- Provide a flexible block plan that can accommodate, and be adapted to, a variety of building types, scales, sizes and a variety of amenity spaces.



Figure: 3.9: Combined walking, cycling, jogging and horse riding in Hyde Park illustrating the aspiration for open space and connectivity in the OA.

Source: GLA

- ↔ New linkages
- New public realm / open space

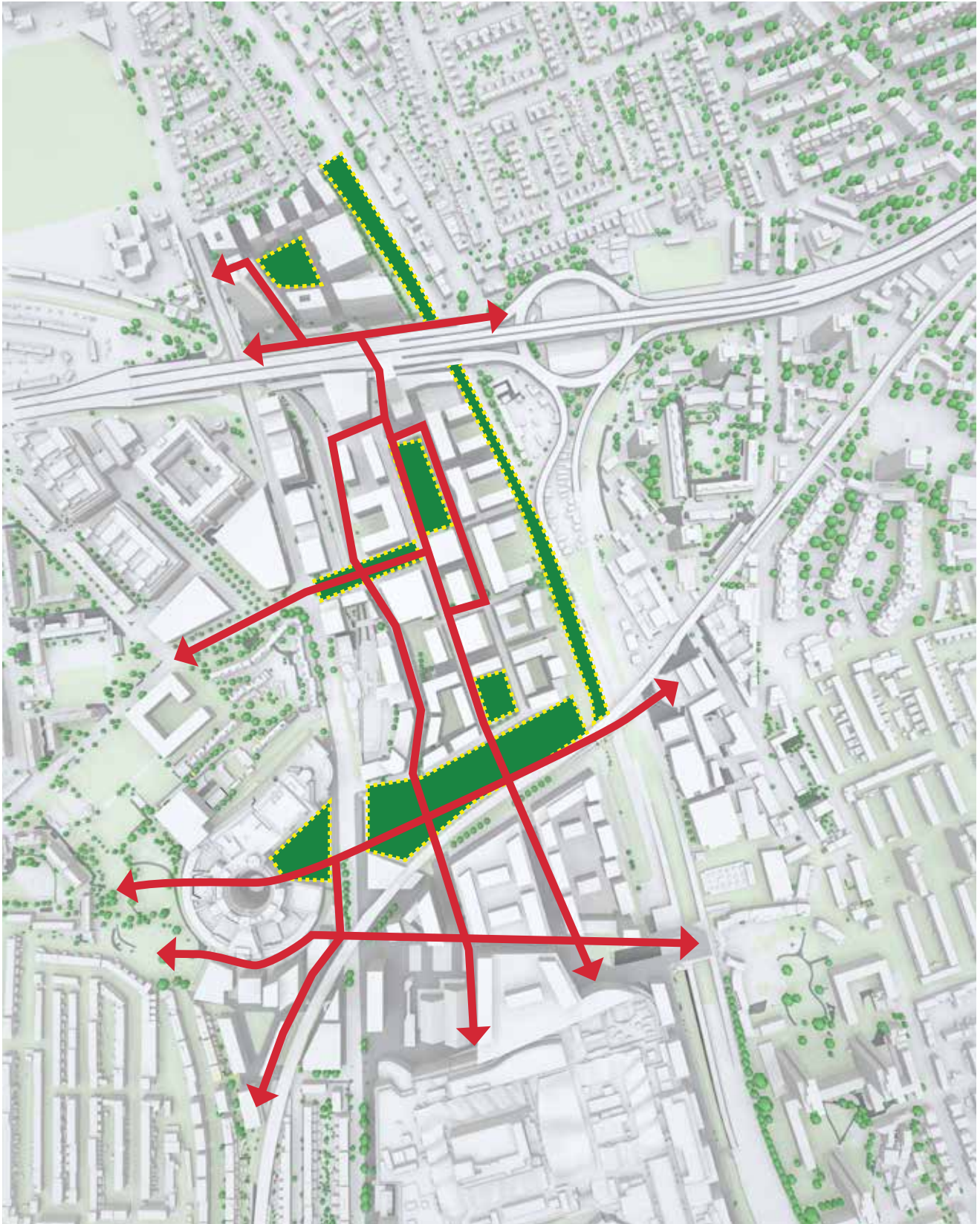


Figure 3.10 Indicative Masterplan

The Indicative Masterplan

3.39 The indicative masterplan illustrates the urban design objectives in White City East. It is intended to provide a broad design framework that would enable individual developments to come forward in a way that encourages them to be linked into a cohesive design solution.

3.40 It should help to optimise the contribution of individual schemes to the layout and design of White City East. It is not intended to be prescriptive, but provides a tool to assist consideration of development proposals.

3.41 The Council and GLA anticipate that individual developers will use this masterplan as a starting point, but will need to undertake more detailed analysis to ensure principles of the urban design objectives are deliverable within their individual schemes, meanwhile cooperating and coordinating with other landowners including TfL and Network Rail to ensure linkages between sites will remain deliverable.

3.42 The indicative masterplan has evolved out of analysis of the three principle design objectives set out earlier in this chapter. It has also taken into account the work undertaken by individual landowners, as they have progressed their proposals for individual sites, to ensure that the masterplan can still represent a model that will assist integration between each development.

New masterplan open spaces

1: White City Green

2: Green link / Ecology corridor

3: Central Line green bridge

4: Imperial West Square

5: BBC TV centre entrance

6: Imperial South Square

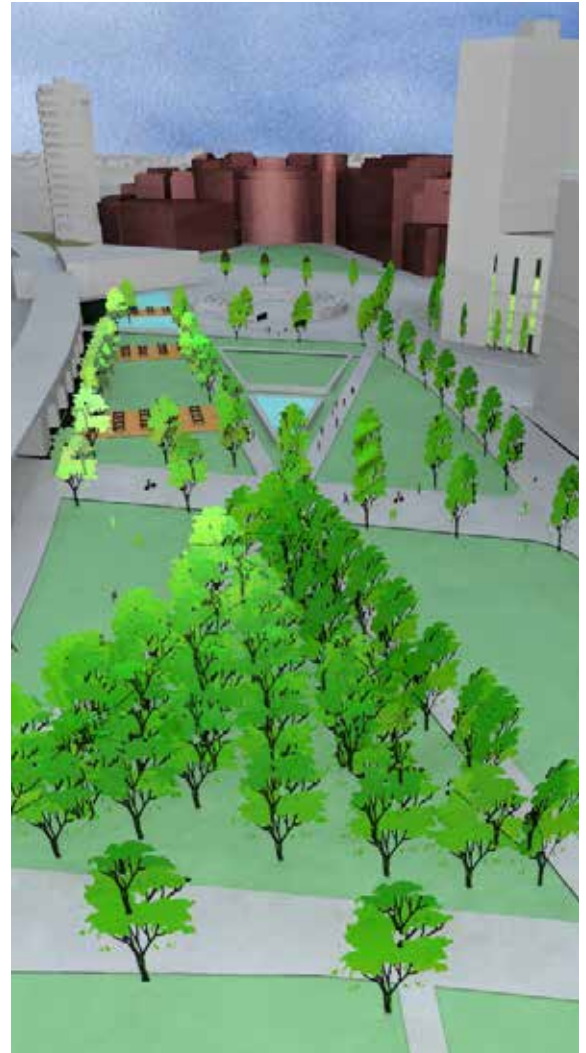


Figure 3.11: Bird's eye view: White City Green (Source 3D model: looking west towards BBC TV Centre Z Mapping)

■ New public realm / open space

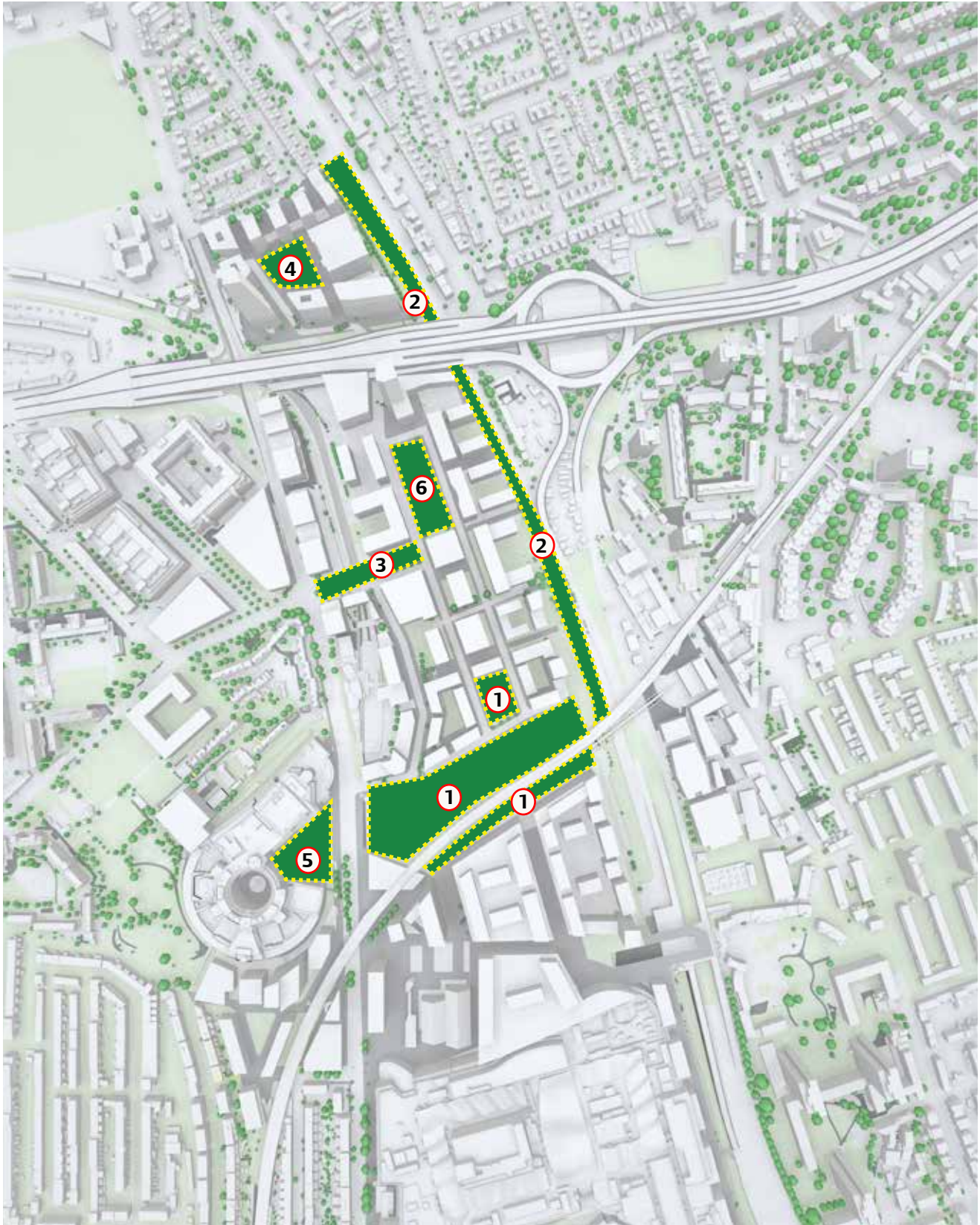


Figure 3.12 New public realm / open space

1. Creating New Public Realm and Open Space

White City Green

Creation of a major new public space

3.43 The Core Strategy specifies that development in White City East must be well related to a pattern of public and private open spaces. Most of this area lies within an area of open space deficiency as identified in the Core Strategy and a new public open space is needed to address this.

3.44 A substantial new public space, White City Green, should be at the heart of the OA, with the preferred location along either side and through the Hammersmith and City Line viaduct, extending east from the forecourt of the iconic BBC TV Centre building up to the West Cross Route. The precise location and shape is a matter for detailed design when development schemes come forward.

3.45 White City Green will attract future development and investment in a part of the OA that benefits from excellent public transport links and provides an ideal point of transition from the retail uses at Westfield to the south with a more commercial and residential mix to the north.

Use of the Green

3.46 While the preferred location of the Green is largely within one major land ownership, it would represent a major asset for all landowners in the OA. Therefore, they are encouraged to take a co-ordinated and collaborative approach to delivering this public open space to a high standard. It could be maintained by the surrounding landowners/occupiers and managed through a body which enables representation from the Council, the local community and landowners including TfL who would also be involved in the process in deciding how that space is used.

3.47 The Green will be bound to the north and south by new commercial, creative, retail, residential and leisure uses and located near Wood Lane Underground Station. It will be convenient not only to new residents but also workers, shoppers and visitors to the area.

Design of the Green

3.48 It will be a high quality area of publically accessible open space which is intended to run on both sides of the viaduct, comprising an area larger than but of a similar shape to Brook Green—a long and narrow public space located between Hammersmith Road and Shepherd's Bush Road

3.49 White City Green will capitalise on the attraction of destinations such as BBC TV Centre and Westfield, as well as being located around iconic structures such as the Hammersmith and City line viaduct and the DIMCO Building. The refurbished BBC TV Centre should open up its central, front courtyard to provide a new forecourt which should be read as a natural extension to White City Green.

3.50 A new and improved crossing over Wood Lane would reinforce this link through appropriate paving and landscaping. The green should enhance overall site legibility throughout the area by delivering a strong sight-line from the edge of the West-Cross Route in RBKC towards the BBC TV Centre and should ensure there is a wide, unobstructed passage between buildings. Views toward the DIMCO building should also be encouraged to provide greater integration with key heritage assets. This is discussed further in the section 'Quality urban design that responds to context'.

3.51 Buildings should be sited sufficiently away from the viaduct and the Green itself, so as not to crowd out this feature or create a canyon effect with excessive overshadowing on to the open space.

3.52 As well as providing soft landscaping, new trees and areas of recreation, the spaces around could incorporate areas for seating, children's play areas, toilet facilities and space for performances or temporary events such as film screenings and speciality market space. Opportunities to incorporate public art and create a setting for temporary cultural events, tying in with local creative industries, could also be pursued.

3.53 The design of the Green must adopt the principles of inclusive design effectively from



Figure 3.13: Precedents for the proposed White City Green



Figure 3.14: White City Green: indicative masterplan including Wood Lane station and Central Line crossing

(Source 3D model: Z Mapping)

the outset, making spaces accessible, easy and convenient to use for all groups of people, with particular attention to thinking about the needs of disabled persons or older people.

3.54 Consideration must be given to the function that this space would provide late at night, with detailed lighting, other 'Secure by Design' techniques and management arrangements to ensure security and safety for all users.

3.55 The Green would also perform an important contribution to the wider sustainable drainage objectives for the OA, with opportunities to provide SUDS within areas of soft landscaping.

Hammersmith and City Line viaduct arches

3.56 The railway viaduct arches that sit centrally in the Green will need to be partially opened up to provide for multiple north-south routes between the two sections of open space, some to be used as cafes and or other activities that could be integrated with the Green. Similar to the viaduct arches in Ravenscourt Park, many of the arches should remain open to pedestrians and cyclists to encourage north-south movement. Particular archways which visually link up to streets running north of the green and from Wood Lane Station should be left open for passage. Areas of hard landscaping could provide a high quality setting for uses such as local cafes, restaurants, shops and community spaces that integrate well with the open space.

Local Open Spaces, public realm and inclusive environments

3.57 Local open spaces are essential to enhancing local character and distinctiveness. The richness of the area will be enhanced by a hierarchy of quality public and private spaces comprising formal parks, allotments, urban squares, communal open space, private open space and wilderness corridors. These will allow for a range of recreational and leisure opportunities which will benefit the health and wellbeing of the community.

3.58 Large scale residential, mixed use and employment uses developments will be expected to consider how their proposals can contribute to the provision of strategic open space to serve the needs of the wider area, particularly the provision of White City Green.

3.59 These will need to include formal and informal play facilities and space for informal ball games for various age groups that are easily accessible to families.

3.60 The creation of successful lifetime neighbourhoods and high quality public realm should be the objective for all developments. Open spaces should integrate features to ensure they are welcoming for people who may otherwise be excluded or feel less comfortable using the space. Spaces should be clearly defined as private or shared amenity space and smaller areas of open space will enhance local character and distinctiveness.

Ecology corridor

3.61 The embankment of the West London Line should provide for an ecological corridor, to encourage further biodiversity in the area and allow movement to and from the Nature Conservation Area which is designated along the embankment to the north of the site at Imperial College. The site has been designated as a green corridor in the Core Strategy and is a SINC (Site of Importance for Nature Conservation) in the London Plan. Denser vegetation along this railway line would also provide benefits by providing a barrier from the busy train line and the noise and pollutants coming from the West Cross Route. Development in the OA should protect and enhance the existing link by creating a publically accessible green route along the length of the OA running from Shinfield Street to the town centre.



Figure 3.15: The shape of White City Green is similar to that of Brook Green



Figure 3.16: The eastern end of White City Green, looking towards RBKC

(Source 3D model:Z Mapping)



Figure 3.17: White City Green from Wood Lane looking to the east

(Source 3D model:Z Mapping)

2. Maximising connectivity

Improved Routes and Connections

Improved routes and connections are needed within the OA, and between it and the surrounding area to overcome major transport infrastructure and large impermeable development sites. The OA must deliver:

- East-west connections under and over major transport infrastructure between White City East, Wood Lane and RBKC; and
- North-south connections from the town centre through as far north as the Imperial College development north of the Westway.

3.62 The Core Strategy policy for White City east states that ‘development must be permeable and well connected both within and outside the area, especially for pedestrians and to overcome the barrier effect of the West London Line/ A3220 and A40’. This OAPF chapter addresses the challenge of connecting the OA to its wider context, drawing communities together, creating links between parks and spaces, buildings, businesses, institutions and destinations and provides options on how these could be best delivered.

3.63 Key to the success of these neighbourhoods will be providing a network of tree-lined routes, streets and paths, bridges and underpasses, open areas to improve legibility; all of which will improve permeability and connections within the OA whilst improving linkages with surrounding communities. Routes that provide easy access to jobs, community and leisure facilities, the town centre and public transport are central to the framework’s design objectives.

3.64 Vehicular, pedestrian and cycle routes will all be delivered, with an emphasis on improving conditions for pedestrians and cyclists whilst reducing traffic congestion. New connections will impact positively by integrating new and existing communities to help with social integration, better access to community, leisure facilities and jobs, encouraging walking and cycling in the area

and promoting a healthy lifestyle by reducing traffic congestion which in turn helps reduce CO2 emissions, air quality, noise and dust.

3.65 New development is expected to achieve the highest standards of accessible and inclusive design. Development must adhere to the principles of lifetime neighbourhoods: places where people throughout their lifetime have access to a range of services, infrastructure and facilities- all easily reachable by foot, wheelchair, mobility scooter, bike and public transport. It will be important to mitigate changes in level and gradients throughout the OA to ensure the provision of step free routes that are accessible to disabled pedestrians.

3.66 TfL and Network Rail own or control areas which are essential to achieving many of these improved connections, including the Hammersmith and City Line viaduct, the Central Line cutting, the Westway and the embankment to the West London Line. They must be engaged as key partners in masterplanning and throughout the application process to ensure the principles in this framework can be delivered.

New and improved connections

- 1: Wood Lane
- 2: New street grid
- 3: Connections through Westfield
- 4: Hammersmith and City Line viaduct arches
- 5: North-South connections under the A40
- 6: Pedestrian bridge over A3220
- 7: Pedestrian underpass of West London Line
- 8: East-west connections over Central Line
- 9: East-west Connections through BBC
- 10: East-West linkages to White City West

Note: Numbers relate to the numbers on Figure 3.18

↔ New linkages

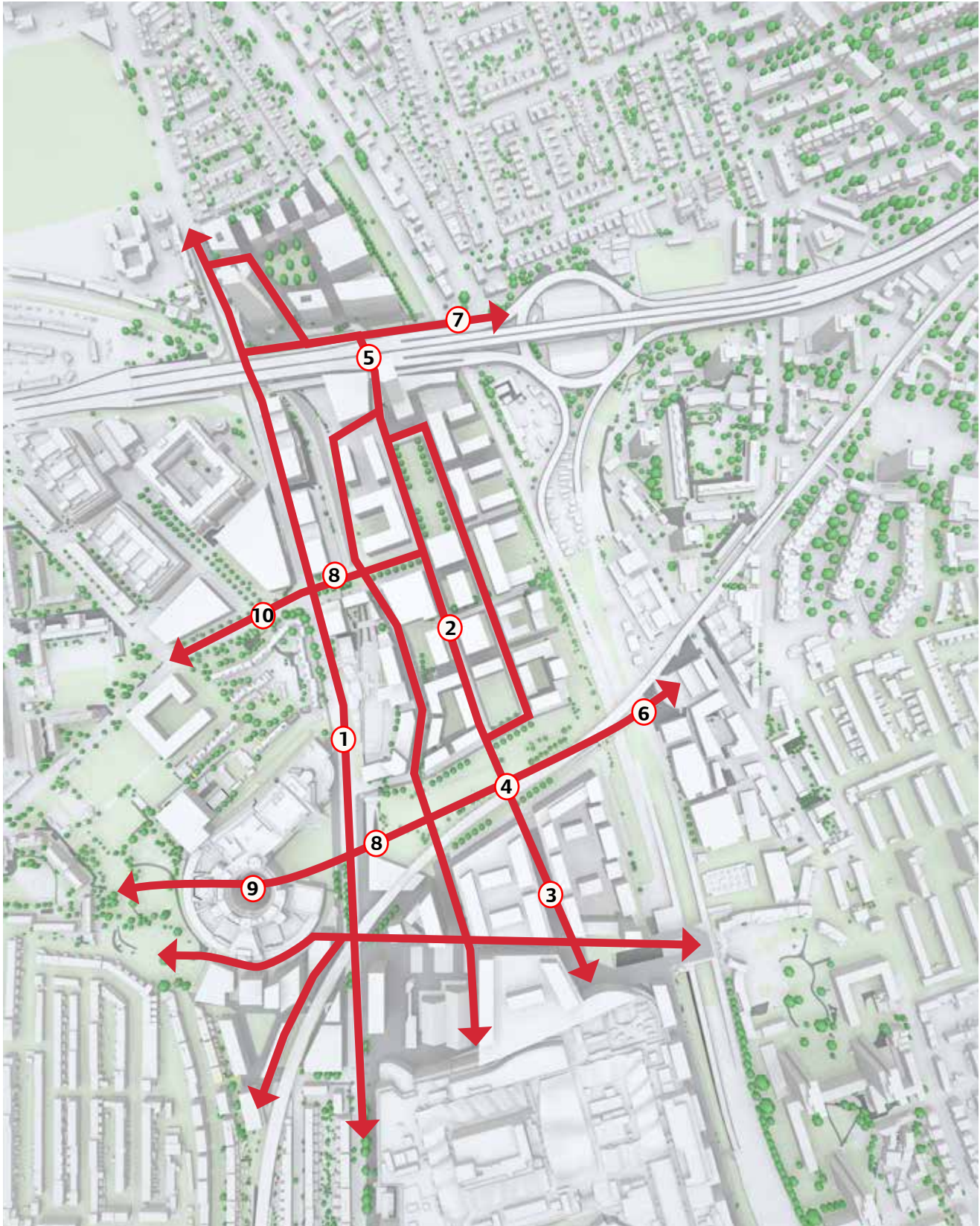


Figure 3.18 New linkages and better connections throughout White City East

North South connections

3.67 Improving north-south connections includes providing a northern entrance to Westfield so that it connects with the area north of the existing shopping centre and encourages movement through White City Green. Provision for the railway viaduct arches to be opened for passage will provide the opportunity to join up to one of two new key routes through the area to the site north of the Westway. New development will be required to create a high quality transition between the town centre and new development areas.

Wood Lane

3.68 Wood Lane is currently the only north/south route that exists in the OA and one which could benefit from improvements to its public realm. The redeveloped BBC TV Centre will enhance the experience for pedestrians along Wood Lane, particularly at the section which will face White City Green.

3.69 A co-ordinated approach should be taken to improve Wood Lane and give the street a clear identity. Wood Lane should become a high profile thoroughfare with active uses to either side. The pavements should be safe and accessible, generous and free of clutter and building frontages should face Wood Lane directly. Where possible, new tree planting should be provided to complement the existing mature plane trees which line part of Wood Lane.

New Street Grid

3.70 Development of the major sites east of Wood Lane will include a new street grid which links into the existing road network to unlock access throughout White City East. The new streets will provide alternative north-south routes to Wood Lane, improve connections with the town centre and capitalise on connecting existing and proposed linkages that run east-west across the area, such as the West Cross Route and the Central Line cutting. New streets will also enhance visual links to help with orientation and site legibility and should align with major roads.

3.71 Figure 3.19 illustrates one road running parallel to Wood Lane from White city Green, underneath the Westway/A40 and into the Imperial College site. It will act as the key connection through the new developments and is referred to as White City Lane in the indicative drawings. The road will reconnect with Wood Lane to the north of the Westway and is intended to be winding at its northern section to discourage motorists from using it as an alternative to Wood Lane, but should include a cycle priority route.

3.72 New streets which have been called Wormwood Way and Reith Row in Figure 3.19 will act as secondary north-south connections, which should not attract through-traffic and therefore will not provide a direct link through the area.

3.73 All the roads should be designed to adoptable standards, be generous to both cyclists and pedestrians and be shaded by trees. Preferably, two north-south and three east-west streets should be provided with two-way traffic, with generous on street parking and street planting with the potential for SUDS incorporated.

3.74 There may be scope for shared surface/ pedestrianised streets within developments; however these must be designed to ensure they are accessible and safe for all users.

3.75 All development proposals must illustrate in their planning applications a clear street hierarchy and demonstrate how the proposed streets connect to the wider network and improve connectivity.

- Wood Lane upgrade
- White City Lane: new road for all users
- Local connection
- Green and ecological route



Figure 3.19: New and improved north-south links

(Source 3D model: Z Mapping)

New North-South connections through Westfield

3.76 The site to the north of the existing Westfield shopping centre is dominated by access roads for commercial servicing, car parking, bus and taxi circulation all of which render the area inhospitable for pedestrians. The northern elevation of the existing building creates a challenge to improving connectivity from the shopping centre to the north, as the public routes through Westfield are two levels above ground (at the same level as the crossing over the West Cross Route). At ground level, the northern elevation is a largely inactive frontage punctuated by servicing entrances. The dominance of roads makes it difficult for pedestrians to make direct east-west connections.

3.77 As a result of this large building footprint and inward looking shopping mall at Westfield, there are limited points of connection from White City East to the Town Centre. Development of the Westfield extension must include ways to address this deficiency, by introducing new passages through the existing development that can provide a connection south from the town centre. This creates a transition from the built up town centre uses, to a more localised shopping centre typology as one moves further north through White City Green towards more mixed use and residential areas.

3.78 Figure 3.21 shows a platform or deck over the existing road network to the north of Westfield which elevates the main pedestrian access to the same level that exists within the shopping centre, as well as the existing link over the West Cross Route into RBKC. This deck provides a development platform for new offices, homes and shops to the north of the existing Westfield and provide for areas of open space. Any decked solution must manage the level changes to integrate any development at deck level with the rest of the OA by adhering to the principles of inclusive design.

3.79 The existing linkage which runs along the West Cross Route adjacent to the existing Westfield development should be improved to encourage pedestrian and cyclist connections with the town centre and how it can connect with the new extension.

Hammersmith and City Line viaduct arches

3.80 The railway viaduct arches in the Green will need to be opened up to provide for multiple north-south openings between the two sections of open space.

3.81 Buildings should be set back from the viaduct and White City Green to respect the important contribution the viaduct makes to the townscape of the White City Green.

3.82 The general scale of development should reflect the building heights set out in Figure 3.57; a taller building close to the West Cross Route, on the axis with the existing East Tower of BBC TV Centre would improve orientation and highlight the crossing point into RBKC.

DIMCO building

3.83 Development of this site should provide high quality public realm and an improved setting for the Grade II listed DIMCO building. The indicative masterplan assumes the retention of the bus stand and bus station but the pedestrian experience and opportunities to appreciate the building could be enhanced through landscaping and public realm design in consultation with TfL Buses and TfL Asset Management.

3.84 The relocation of the bus stand located in the DIMCO building and the adjacent bus station could be explored but only if they could be accommodated on a site south of the H&C line viaduct and with the agreement of TfL Asset Management.



Figure 3.20: Illustration of public realm and shops within the approved Westfield extension

(Source: Westfield)



Figure 3.21: Westfield High Street: Indicative masterplan

(Source 3D model:Z Mapping)

North-South connections under the A40/ Westway

3.85 The Westway changes from an at-grade road to become elevated at the north eastern corner of the OA. The A40 slopes up to a point that it is high enough to provide parking and storage areas for logistics companies underneath the elevated road infrastructure. Much better use of these spaces in the OA will ensure scarce land resources are used to their optimum potential and help improve north-south links between uses such as Wormwood Scrubs.

3.86 Given the provision of social, leisure and community facilities such as schools, Hammersmith Hospital and Wormwood Scrubs to the north of the OA and the need to give access to these from the existing and proposed communities to the south, improved connections along the length of the A40 are a key element of this framework and urban design strategy.

3.87 The underside of the Westway will be primarily used to deliver a major north-south route for vehicles, pedestrians and cyclists (see mark 3 on Figure 3.22). The remaining space that is not used for the main route could be landscaped and managed in a way that provides leisure opportunities, community use or possibly commercial use. Precedents along the Westway of well-maintained, overlooked and attractive recreational spaces could be emulated here to complement new development on sites north and south.

3.88 The entrance to an east-west pedestrian subway (see figure 3.22-Point 3) should connect to the new north-south routes under the Westway to ensure that people coming from the south of the OA can easily find and make the connection to the east, and vice versa. These two connections must work together to improve overall accessibility in the OA.

West of Wood Lane

3.89 To the west of Wood Lane, there is an existing tunnel that provides an east-west connection along the A40. The access route to the tunnel along the southern edge of the A40 is poorly lit and suffers from a lack of visibility through the tunnel. Any development under the Westway in this location should seek to improve the sense of safety, security, overlooking and quality of the environment along this route. (see figure 3.22 Point 1).

3.90 At Bloemfontein Road, the surface crossing leads to a poor quality footbridge over the Central Line to Du Cane Road. This footbridge should be improved to facilitate a safe, attractive route from Bloemfontein Road to Hammersmith Hospital, the Scrubs and Linford Christie Stadium.

3.91 Development should enhance overall site legibility, delivering strong sight-lines to help orient pedestrians, by providing separate blocks with open streets and passageways that will include views north to south along new roads and cycle/ pedestrian connections from the Imperial College development, south towards the White City Green, through viaduct arches and through the Westfield extension site.

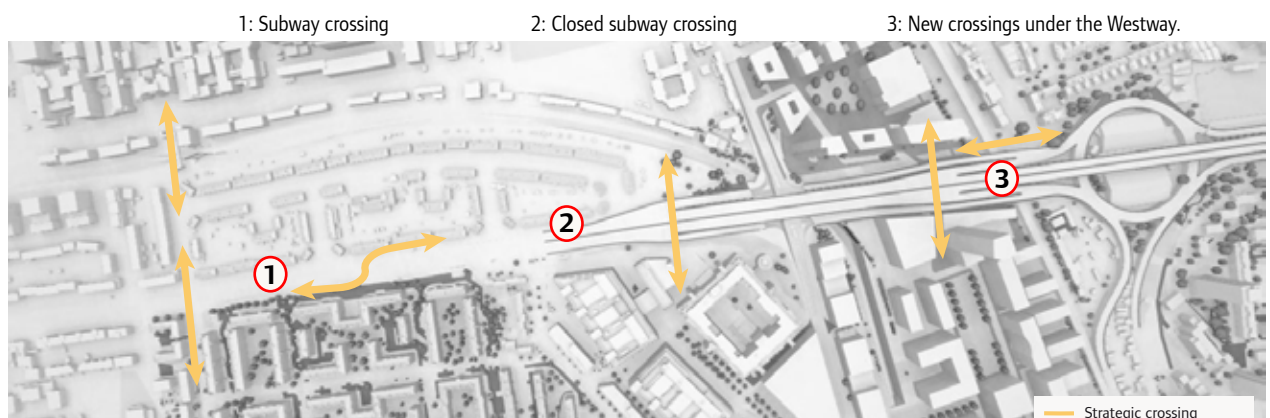


Figure 3.22: Westway / A40: Key north-south connections

(Source 3D model:Z Mapping)

- Pedestrian & cycle tunnel
- Strategic pedestrian link
- Potential for pedestrian areas
- Strategic green link



Figure 3.23 Indicative masterplan: under the Westway / A40



Figure 3.24: Playspace and horse riding already under the Westway roundabout in RBKC

East-West connections

3.92 Connections that will run over or under the West Cross Route and West London Line are essential to link communities from the east in RBKC with the White City development area and transport connections, and also provide vital pedestrian and cycle linkages from this area to access areas towards central London. Improved links from White City east to White City west are also required to link the residential estates to the new development.

3.93 A redeveloped BBC TV Centre and the development of the White City Green could improve the experience for pedestrians along Wood Lane by removing the existing impermeable gated frontage to the BBC TV Centre and could encourage further linkages through the site towards Hammersmith Park.

3.94 Solutions for providing improved connections over the Central Line cutting are of upmost importance for the success of the new development in White City east. New and wider bridges to the north and south of White City east

will be required in order to be able to access and service the amount of development required in the area.

Pedestrian Bridge to the east

3.95 The existing crossing over the West Cross Route was recently built as part of the Westfield centre development, which connects the Edward Woods Estate with the north-east corner of Westfield. It lands on the eastern side of Ariel Way where roads dominate and therefore does not provide a useful pedestrian link other than to those who are able to walk along the west cross route, towards the town centre.

3.96 An elevated pedestrian footbridge over the West London Line and the West Cross Route could be provided immediately to the south of the Hammersmith and Fulham viaduct. It would land in the part of the Green which would provide a direct pedestrian and cycling route into RBKC from the open space. The possible eastern landing point would be to the north of 'Notting Dale' development in the OA, see mark 2 on Figure 3.35, 3.27 and 3.29.

← New crossings over and under the West London Line

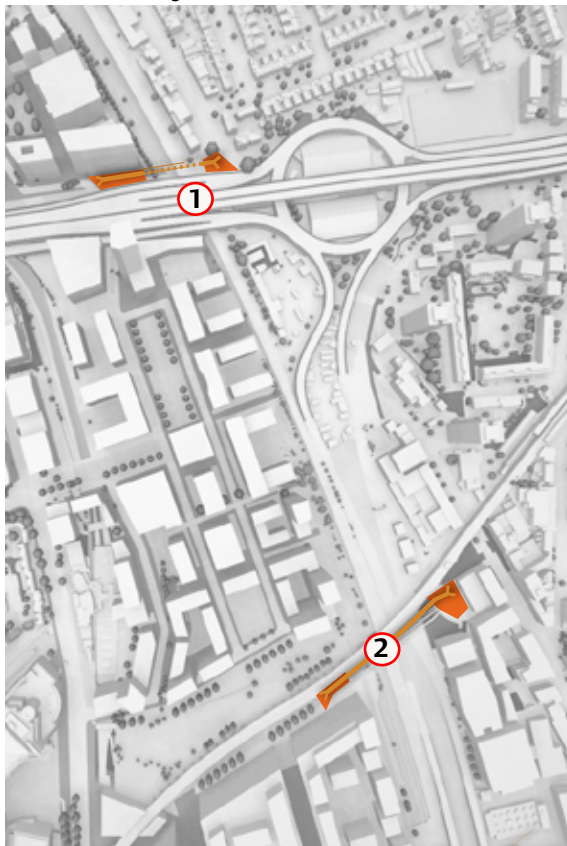


Figure 3.25: West Cross Route and West London Line: key connections (Source 3D model:Z Mapping)

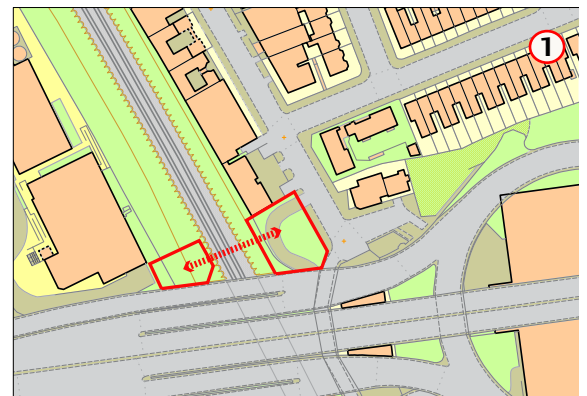


Figure 3.26: Landing site for Northern crossing

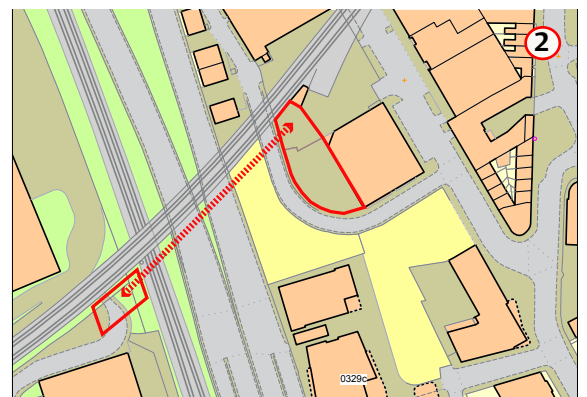


Figure 3.27: Landing site for Southern crossing

3.97 RBKC also has a long-term strategy for the Latimer area that places importance on introducing connections to White City. It has undertaken feasibility studies for achieving better east-west connections and has identified this bridge as one of two preferred options which should be delivered as part of development in the OA.

3.98 The design of any bridge crossing should be simple, elegant and striking in appearance, with careful attention given to its landing on the eastern side and how it connects with key north-south connections through the viaduct archways.

Pedestrian and Cycling Underpass- Imperial West to the Westway Sports Centre

3.99 The Westway Development Trust has developed a cycle and pedestrian link along the section of the Westway in RBKC which terminates at the boundary with LBHF. Both RBKC and the Westway Trust have aspirations to extend the connection westwards.

3.100 A shallow underpass to the north of the A40 would provide a route for pedestrians and cyclists beneath the existing West London Railway

line, linking the Imperial College London site with RBKC near the Westway Sports Centre, see mark 1 on Figure 3.25, 3.26 and 3.28.

3.101 The Imperial College London development should integrate into their design an entrance for the underpass, one which provides a gentle gradient into the underpass to ensure visibility to ensure safety and security, along with CCTV and lighting where necessary.

3.102 Development should enhance site legibility, delivering strong sight-lines to help orientate pedestrians. Figure 3.21 shows a layout for the Westfield extension that will enhance the visual link from the current crossing point which runs from Hunt Close to Ariel Way over the West Cross Route, across the site towards the east tower at the BBC TV Centre. The site layout should ensure there is a wide, unobstructed passage between buildings at this location.

3.103 Views should run east-west along the Hammersmith and City line viaduct from the West Cross Route toward the BBC TV Centre as discussed in the open space section.



Figure 3.28: Northern crossing (1): Shallow pedestrian underpass

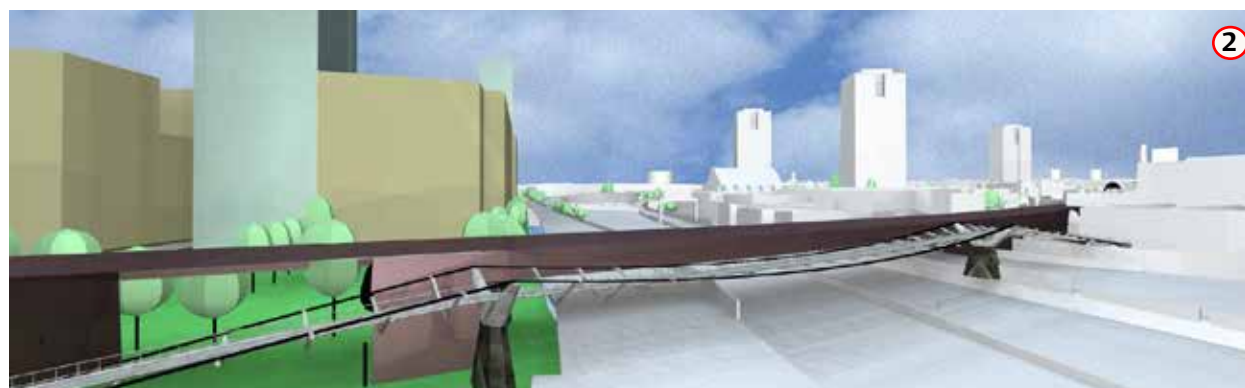


Figure 3.29: Southern crossing (2): impression of pedestrian and cycle link

(Source 3D model: Z Mapping)

East-West connections across the Central Line

3.104 The Central Line cutting, as shown in Figures 3.30 - 3.33, restricts access from Wood Lane to the part of White City East between the A40 and the Hammersmith and City Line Viaduct to two narrow bridges. The transport chapter identifies the need to improve bridge access to provide the necessary access and circulation into this area. This could involve straightening, widening and provision of generous sized pedestrian routes over the cutting. Any bridge works or new bridges would have to be delivered with the least possible disruption to railway services. Precedents for a similar approach can be found at Bishop's Place over the Network Rail cutting into Liverpool Street (Bishopsgate, LB Hackney).

Southern bridges- Former Marks and Spencer site

3.105 The existing road bridge to the former Marks and Spencer site at the south is narrow and provides limited scope for further pedestrian and vehicular access, while the walls along the pavement limit visual links. A new wider, lightweight bridge structure over the Central Line cutting would improve the visual and physical links from Wood Lane and the TV Centre forecourt, see Figure 3.32 and mark 3 on Figure 3.33.

3.106 A new additional bridge over the cutting to the south of the existing road bridge should also be considered as it would provide a direct connection to the forecourt of the BBC TV Centre. It should be treated as an extension of White City Green and will provide important public realm and physical and visual links from within the Green to Wood Lane and the BBC TV Centre. Given operational requirements of the Central Line, it may be impractical to fully cover the cutting as opposed to bridging over parts of it. The suggested bridges are envisioned as wide, light-weight structures for pedestrian and amenity use. The delivery of this link will need to be provided and considered in light of future development proposals for this site, see Figure 3.32 and mark 3 on Figure 3.33.

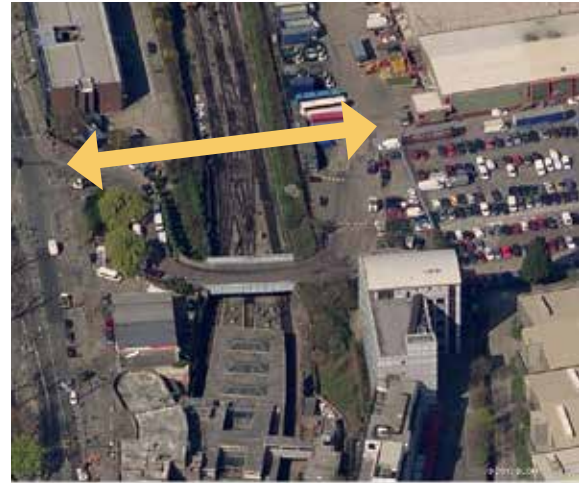


Figure 3.30: Crossing 1 direct link to White City East from South Africa Road



Figure 3.31: Crossing 2 direct link to east side White City station



Figure 3.32: Crossing 3 direct link between White City green and BBC TV centre and Wood Lane station

- Pedestrian & cycle bridge
- Vehicular bridge
- Strategic pedestrian link
- Route through Tube station

- ① Northern bridge - Depot Road
- ② White City Station: Ticket hall entrance
- ③ Southern bridge - Marks and Spencer site



Figure 3.33: Former Marks & Spencer Site / St James Central Line cutting: Key connections

(Source 3D model: Z Mapping)

Northern Bridge- Depot Road

3.107 At the junction of South Africa Road and Wood Lane is an existing access road bridge, along Depot Road. This access road does not provide a direct visual link over the Central Line cutting into White City east as it bends to the right after the crossing with Wood Lane.

3.108 Figure 3.33 shows a new bridge that extends straight across the cutting after crossing the junction with Wood Lane to provide a strong visual and physical connection from South Africa Road into White City east and provide further vehicular and pedestrian capacity. The masterplan shows a landscaped area over a lightweight structure, which could join the new bridge up with the existing to provide a wide opening towards White City East. See mark 1 in Figure 3.33



Figure 3.34: Potential provision for an additional entrance at Wood Lane Station Source: TFL

White City Station- Ticket Hall Entrance

3.109 A new eastern ticket hall for the White City LUL station could provide an additional connection to Wood Lane over the Central Line Cutting. Whilst this is not an open and public route at this point in time, it could provide direct access into the tube station if development of the Centre House site was to be delivered.

3.110 To gain access to the station platforms and to cross the railway, the existing walkway would need to be re-built or reinforced to be appropriate for public use. See mark 2 in Figure 3.33.

Wood Lane Station Entrances

3.111 The introduction of improved public realm and open space to the east and south of Wood Lane Station as part of 'White City Green' creates an opportunity to improve access from the station to White City Green.

3.112 The development of the Green should consider how it could help deliver a southern entrance to the station and enhance access into the northern entrance directly from the Green. High quality public realm around these new entrances will be an essential ingredient to any improvement works.



Figure 3.35: Potential provision for an additional entrance at Wood Lane Station Source: TFL

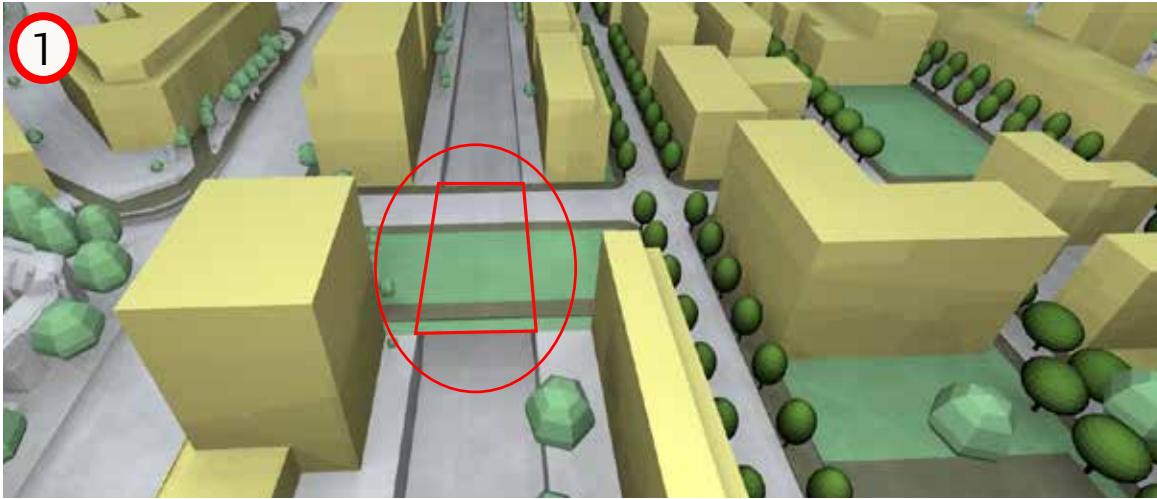


Figure 3.36: Wider bridge over the Central Line cutting: South Africa Road to White City East (Source 3D model:Z Mapping)

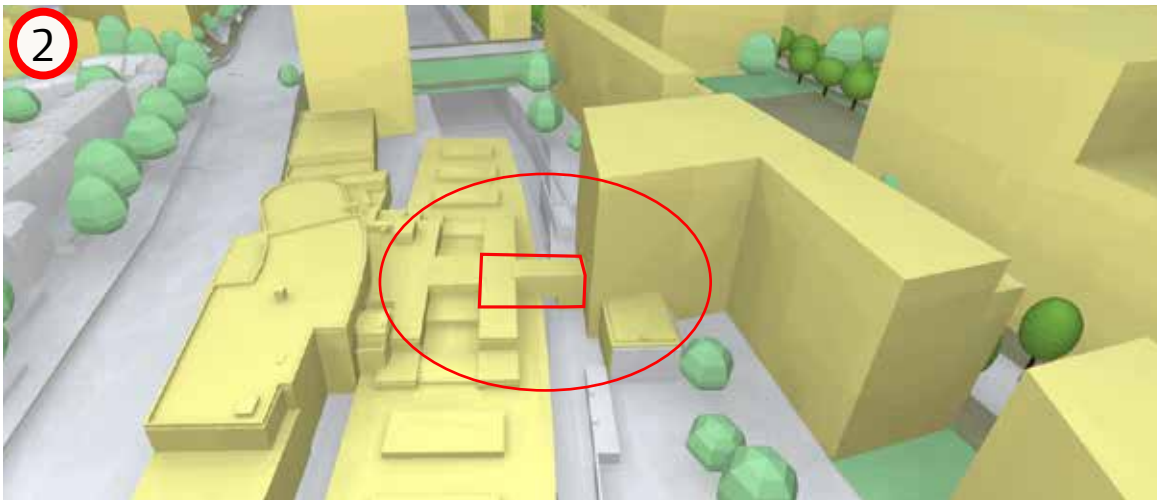


Figure 3.37: Eastern ticket hall and pedestrian walkway at White City LUL station (Source 3D model:Z Mapping)

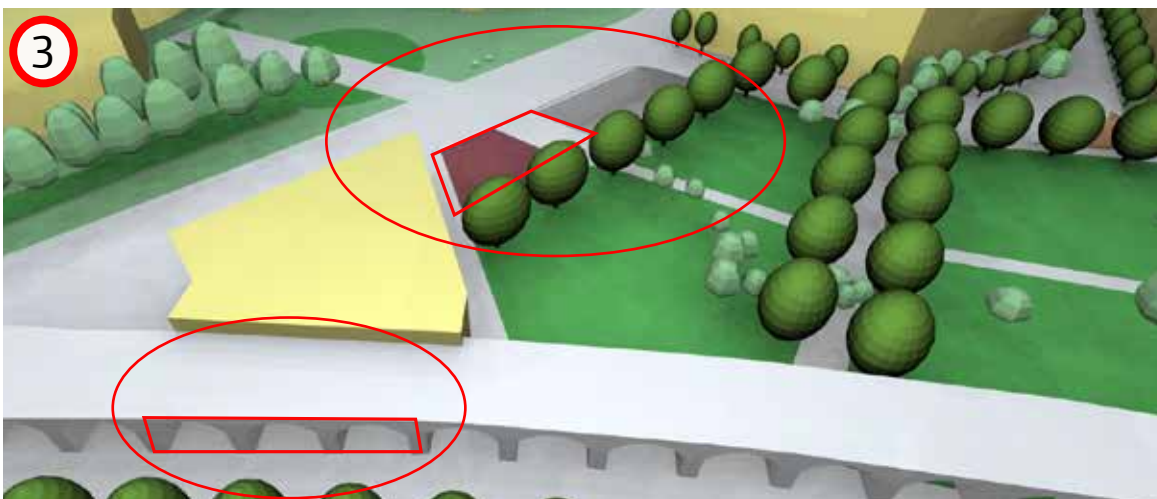


Figure 3.38: Lightweight bridge north of Wood Lane LUL station and new southern entrance to Wood Lane LUL station (Source 3D model:Z Mapping)

East-West linkages through the BBC TV site

3.113 The priorities for the BBC TV site include:

- Opening up the forecourt of the building to create a welcoming public space that serves as an extension of the White City Green;
- Creating a pedestrian route with active frontages around the perimeter of the studio ring into Hammersmith Park, to reflect the unique geometry of the site;
- Introducing public routes through the building to Hammersmith Park without damaging historic fabric and plan form of the listed building; and
- Providing two new routes through the viaduct arches to improve connections to Hammersmith Park and provide the opportunity for the north-

south link along the Hammersmith and City Line viaduct to Shepherds Bush Market.

Pedestrian route from BBC TV Centre to Shepherds Bush Market

3.114 Two new routes through the viaduct arches should be introduced to improve connections to the BBC TV Centre site from Wood Lane and White City Green. The opening of these arches could present the opportunity to link with a pedestrian and cycle route alongside the Hammersmith and City Line viaduct from Shepherds Bush Market London Underground Station towards the BBC TV Centre site. This route could provide the opportunity to open further arches along the viaduct for offices and active use where it is not seen to negatively impact on amenity of nearby residents.



Figure 3.39: BBC TV Centre:



Figure 3.40: BBC TV Centre, Studio 1 and main entrance



Figure 3.41: BBC TV Centre, Inner courtyard

- █ Pedestrian areas
- █ Public Park
- Opening up BBC TV Centre
- Existing mature trees
- █ Pedestrian & cycle bridge
- Strategic pedestrian link
- █ Pedestrian route
- Route through BBC TV Centre

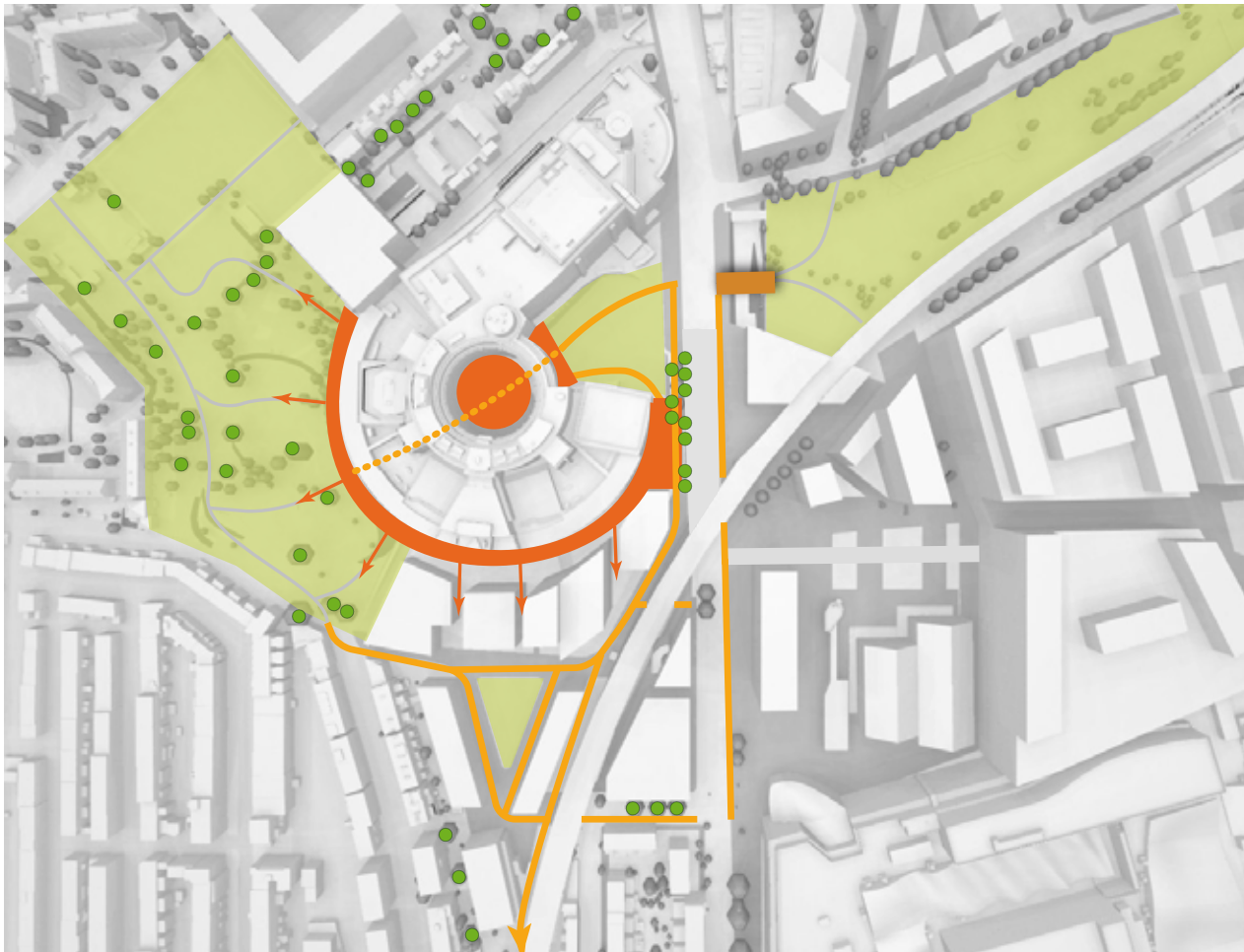


Figure 3.42: BBC TV Centre, Indicative masterplan

(Source 3D model: Z Mapping)



Figure 3.43: Future BBC TV Centre development

(Source: BBC)

East-West linkages in White City West

South Africa Road

3.115 Any future development of the BBC Media Village site and the TA Centre should ensure permeable sites to allow pedestrians to connect easily with the wider area and create a transition from the residential areas of the estates to the commercial development along Wood Lane. Development of these sites should be accompanied by a unified public realm with a consistent approach to pavements, tree planting and street furniture. The existing area of open space and its group of mature trees to the north of the Wood Lane estate along South Africa Road could provide the focus for the public realm strategy.

Commonwealth Avenue

3.116 Minor alterations to the servicing arrangements of the Canberra School would enable Commonwealth Avenue to achieve a pedestrian and cycle connection eastwards to Dorando Close, giving estate residents more direct access to the resources, transport connections, and amenities in the east of the OA.



Figure 3.44: Wood Lane



Figure 3.45: South Africa Road



Figure 3.46: Commonwealth Avenue



Figure 3.47: Dorando Close

- Strategic pedestrian link
- Improved existing roads
- Pedestrian areas
- Strategic green link

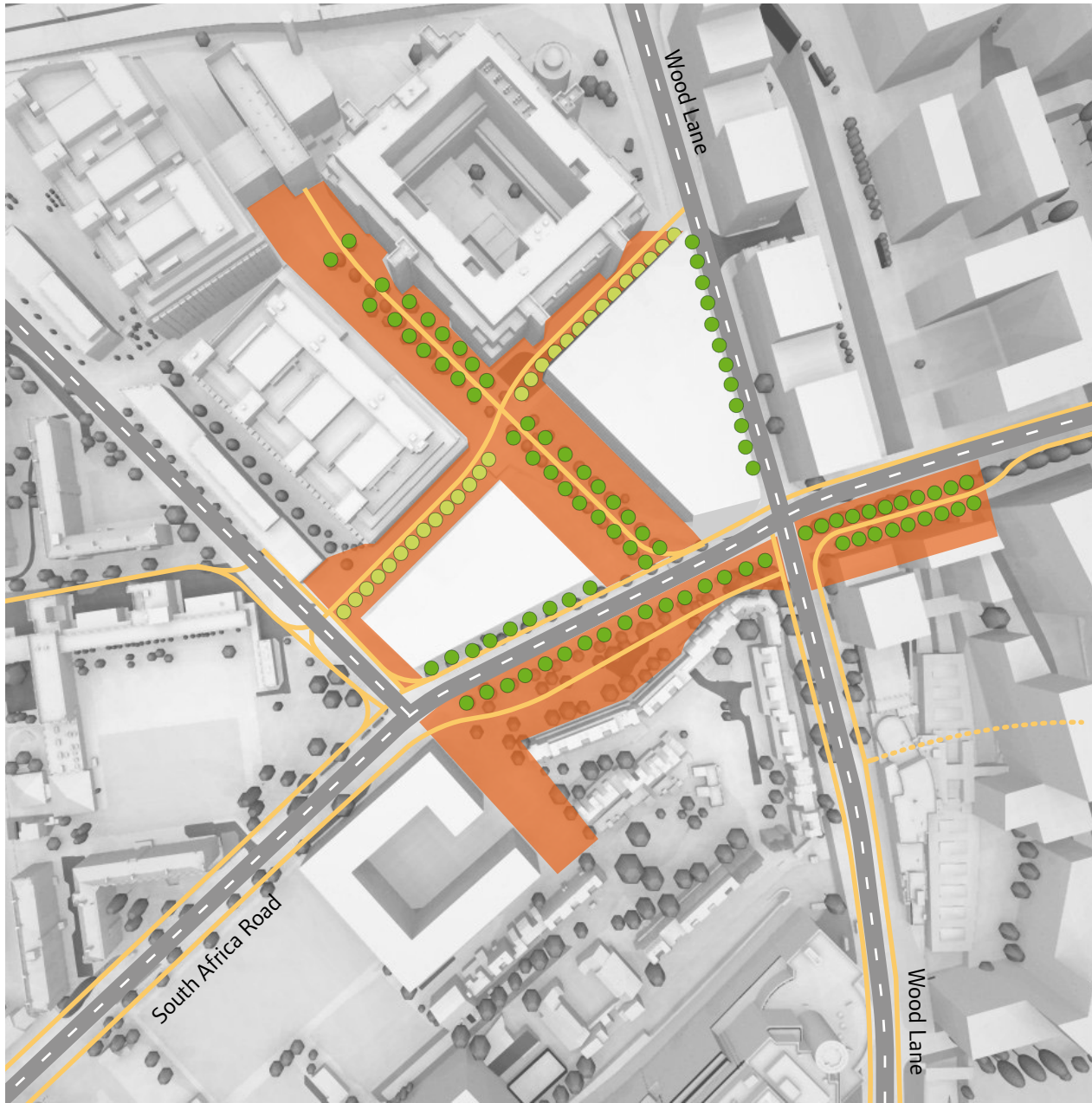


Figure 3.48: Indicative Masterplan: Improved gateway to the White City Estate

(Source 3D model: Z Mapping)

3: Quality urban design that responds to context

Streets and Buildings that create a sense of place

White City East will be expected to deliver flexible, mixed use neighbourhoods to accommodate high quality housing, community facilities, local shops and employment space.

The majority of residential development should be in the form of modern mansion blocks (normally between 6-10 storeys) with shared amenity space at the centre of the block along with private amenity space on balconies and roof terraces.

Commercial buildings should be located close to transport connections and should be provided in mixed developments with active uses at ground floor, and high-quality buildings that provide a layout that is flexible for a variety of commercial and related uses.

New retail development should be provided in an open environment at ground floor level on streets.

Developments should provide a legible environment that relates to, and connects with, the setting and context of the surrounding urban grain and character.

Creating Great Neighbourhoods

3.117 White City East should feature good quality residential developments with a mix of housing types for people on all incomes. This principal is about creating human scale, sustainable residential areas where a wide range of people will want and be able to live and take pride in the area. It requires consideration of a range of factors, including the mix of housing types and sizes, and building heights. This is fundamentally about what makes a place work as a living environment, such as the relationship between dwellings, streets, open spaces, local facilities and public transport, and ensuring that people feel safe and are safe in the area.

3.118 All new housing areas must be designed to provide safe, well connected, inclusive, permeable and liveable neighbourhoods capable of accommodating a range of households and assimilating different tenures, with access to supporting facilities and adequate public and private open space.

3.119 The design of affordable housing should be tenure blind in terms of the external appearance of the buildings and amenity. Buildings that include affordable housing should be integrated in the overall residential scheme. The London Plan (para 3.76), the Housing SPG (para 4.4.34) and the LBHF Core Strategy requires affordable housing and market housing to be integrated in any development and to have the same external appearance.

3.120 The majority of new housing should be provided in mid-rise, modern mansion blocks or stacked maisonettes reminiscent of terraced housing. There may be the opportunity for new housing in the limited number of tall buildings which are identified in the next section. There is scope for relatively high density (700-900 HRH) housing but densities in individual schemes will be assessed having regard to the London Plan, the LBHF Core Strategy (H3) and the LBHF Development Management Local Plan (DMLP) housing and design policies and standards.

3.121 Mansion blocks should have frequent shared entrance cores, or in the case of maisonettes, their own front doors. Developments will be expected to incorporate shared amenity space, along with private amenity space on balconies and roof terraces.

3.122 Homes should open onto tree-lined streets with on-street parking and generous pavements. In some cases, local shops and community facilities at ground floor level will provide active frontages and safer street environments.

Retail, Commercial and large spaces

3.123 Commercial office buildings could be of a similar form to the Monsoon Accessorize headquarters, being medium-rise buildings with a

striking design and flexible floorspaces that allow for a range of commercial tenants.

3.124 Commercial office buildings could also be delivered in mixed-use buildings with active uses at ground floor and a mix of non-residential community uses throughout. New commercial buildings for creative, media and technology should be located near public transport and closer to Wood Lane.

3.125 Town Centre uses will be extended north of the existing Westfield with major leisure, residential and retail in an active environment with a series of retail uses at ground level on streets with offices and residential above. New development should avoid the monolithic approach of the existing Westfield Mall and instead deliver variation in scale, grain and built form to make a successful transition from the shopping mall typology to a high street typology and encourage a suitable mix of commercial and residential uses. This will require a finer grain of development requiring separate blocks, open public streets and passageways without canopies or bridges above and generous open spaces.

3.126 The BBC TV Centre could be refurbished for new leisure, commercial, academic or other institutional uses.

3.127 The indicative masterplan is intended to provide a reasonably flexible framework within which a variety of uses could be located. The design and location of large buildings will need to be undertaken with care to respect the urban grain.



Figure 3.50: Artist impression of residential use on the Westfield site



Figure 3.51: Artist impression of Imperial College student accommodation
(Source: AukettFitzroyRobinson)



Figure 3.52: Artist impression of Imperial College site from Westway



Figure 3.49: Artist impression of retail in the Westfield site
Source: Westfield



Figure 3.53: Commercial blocks / Monsoon Accessorize headquarters
Source: RB K&C

Improving the setting of Listed Buildings

The departure of the BBC from the Grade II listed Television Centre creates opportunities to open up the large, inward looking site, improve the public realm surrounding the site and provide new high quality buildings that will enhance the setting and views towards the BBC TV Centre.

Development of the Westfield Extension should look to improve the setting of the Grade II Listed DIMCO building and provide high quality public realm around its base and improved views towards the building.

3.128 The central ring building and Studio 1 of the BBC with its iconic wall to Wood Lane are of special architectural and historic interest at a national level; and parts of the drama block and older ring of studios are of interest at a local level. The elements of special architectural and historic interest should be retained and any proposals for re-use should improve their setting and provide opportunities for the public to appreciate them through the introduction of new uses compatible with the character of the building.

3.129 Subject to satisfactory justification being provided, a refurbishment and redevelopment scheme involving removal of the ancillary buildings which are not of special architectural or historic interest could help make better use of the site, improve the access to the site and improve the setting of the buildings of highest significance. Any proposals for new development should be part of a wider masterplan for the whole TV Centre site, which takes into consideration the setting of the most significant elements of the building and ensures they are incorporated appropriately.

3.130 There may be an opportunity to redevelop some of the ancillary buildings to the main structure which are not of special interest. These include the majority of the drama block, the yard used for satellite dishes, the east tower, the multi-storey car park, the restaurant block and the 1990s addition to the TV Centre on Wood Lane.

3.131 Development on the service yard to the south of the round building must be sensitive to the scale and privacy of the adjoining terraces and should be of modest scale and in keeping with terraced housing to the south.

3.132 Development on the site of the drama block should be of a scale that mediates between the terraced housing to the south and the TV Centre building. This building should include an opening to allow an access route into Hammersmith Park.

3.133 The existing east-tower has a poor presence at ground level and provides little contribution to the local townscape; its demolition would improve the setting of the listed building. Its replacement with a building of a similar scale with an improved design, location and orientation could make a positive contribution to the legibility of the public realm, as set out in the next section. New development must have an active, well considered presence at ground level to Wood Lane and should incorporate an improved public realm with connections to the wider site, as discussed in 'Wood Lane, North South connections'.

3.134 The restaurant block provides another opportunity to introduce a new building with an active frontage to Hammersmith Park. The site relates primarily to the park and low-rise character of the Wood Lane estate. Development should therefore be sensitive to this context and its impact on the park and should maintain a domestic scale.

3.135 There may also be scope to improve the 1990s extension to the TV Centre to provide a more active frontage to Wood Lane at ground floor level. New buildings and extensions should not compete with the elements of TV Centre that have special significance but should enhance the setting.

3.136 The multi-storey car park makes a negative contribution onto Wood Lane. Redevelopment of this site should introduce a building with active frontages on Wood Lane, and mediate between the scale of residential streets to the south and west and the large, institutional building of TV Centre.

3.137 The BBC TV Centre could be refurbished to include a mix of new leisure, commercial, academic or other institutional uses along with some residential space.

DIMCO Building

3.138 New development should provide a high quality public realm and setting around the Grade II Listed DIMCO building.



Figure 3.54: BBC TV centre

Source: GLA



Figure 3.55: Restored DIMCO building

Source: GLA

Building heights in the opportunity area

The majority of new buildings should be medium-rise of 6-10 storeys with some lower-rise 3-6 storey terraces, particularly in areas close to existing neighbourhoods.

Some buildings of 11-15 storeys may be appropriate along the A3320 and around Westfield's new shopping street. The existing 15-storey tower at the BBC TV Centre could be replaced with a better-designed tower that subject to detailed design considerations, could be taller than the existing one.

16-20 storey buildings may be appropriate at a point of townscape significance such as the north-east corner of White City Green.

Slender towers of approximately 21-30 storeys (up to 100 metres) could be appropriate along the Westway.

The acceptability of particular proposals for tall buildings will be considered in detail at planning application stage in accordance with the development plan and having regard to the urban design objectives of the OAPF.

3.139 The LBHF Core Strategy policy for WCOA states that 'The council will expect most new development to be low to medium rise, however a limited number of tall buildings of exceptionally good design may be acceptable, in particular, close to the A40 and A3220 and in any other areas identified in the White City OA planning framework tall buildings strategy.' Policy BE1 also says that tall buildings may be appropriate 'in masterplanning which is consistent with the SPD'.

3.140 Tall buildings are generally defined in the London Plan as 'those that are substantially taller than their surroundings, cause a significant change to the skyline or are larger than the threshold sizes set for the referral of planning applications to the Mayor' (the threshold for referral is 30 metres). The LBHF Core Strategy glossary defines tall buildings as 'those that are substantially taller than their

neighbours and/or which significantly change the skyline.'

3.141 The OAPF gives guidance on building height and the possible general locations for tall buildings. The purpose of the indicative masterplan is to illustrate how that guidance could be applied but not definitively propose or prescribe the height of buildings in particular locations. The masterplan shows approximate locations where tall buildings may be acceptable subject to a full appraisal at the time when a planning application is made, particularly with regard to precise location, height, design and impact.

General Guidance

3.142 Well-designed tall buildings can make positive contributions to townscape and can play a valuable role in place shaping. They can provide variety and interest, articulate a point of significance for the wider area, such as deliberately framed views and vistas, highlighting regeneration areas and help with wayfinding through identifying landmarks.

3.143 The approach to building height is derived from and complements the public realm and open space objectives, analysis of the built and heritage context and the desktop views assessment using a 3D model. The preferred height and scale of development within the OA varies depending on the immediate context and new development must ensure a variety of scale, massing and architecture is delivered across the OA. All development must relate appropriately to neighbouring buildings, public open space and vehicular, pedestrian and cycling routes.

3.144 The indicative masterplan identifies the limited locations where taller buildings would be suitable and could contribute to the legibility of the wider area without unreasonable overshadowing of strategic open spaces or nearby residential areas. An analysis of local and wider views, including views from within conservation areas within LBHF and RBKC has also informed this approach and requires that the introduction of tall buildings in preferred locations should have little detrimental impact on views. A detailed views analysis which considers the cumulative impact of building height in the area will be required to be submitted with each planning application which comes forward.

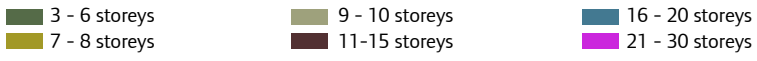


Figure 3.56: Building heights in the indicative masterplan

(Source 3D model: Z Mapping)

Preferred Locations for tall buildings

Along the A40/ Westway: Gateway to London

3.145 The OA acts as a transition zone between western and central, inner and outer London. Tall buildings near the Westway and northern section of the West Cross Route will provide a strong identity for the OA and provide beacons for regeneration. They will identify and landmark White City as the 'gateway' into central London, echoing similar clusters along the rest of the route. Taller buildings will provide a buffer to the flyover for development within the OA, and the scale would balance and respond to the existing height of the elevated road infrastructure.

3.146 The indicative masterplan indicates scope for up to two towers (approximately 30 storeys or 100 metres) in the general area around the Westway: one to the north and one to the south of the Westway. Tall buildings in these locations will provide points of visual emphasis on the north-south roads through the site and will be landmarks in views northwards from White City Green and Westfield.

3.147 Any tall buildings must be located sensitively to limit the impact of overshadowing on existing and future communities or public open space, to avoid the canyon effect and take into consideration any impact on views of the nearby conservation areas.



Figure 3.57 Aerial view of the Westway - existing

(Source: Westfield)

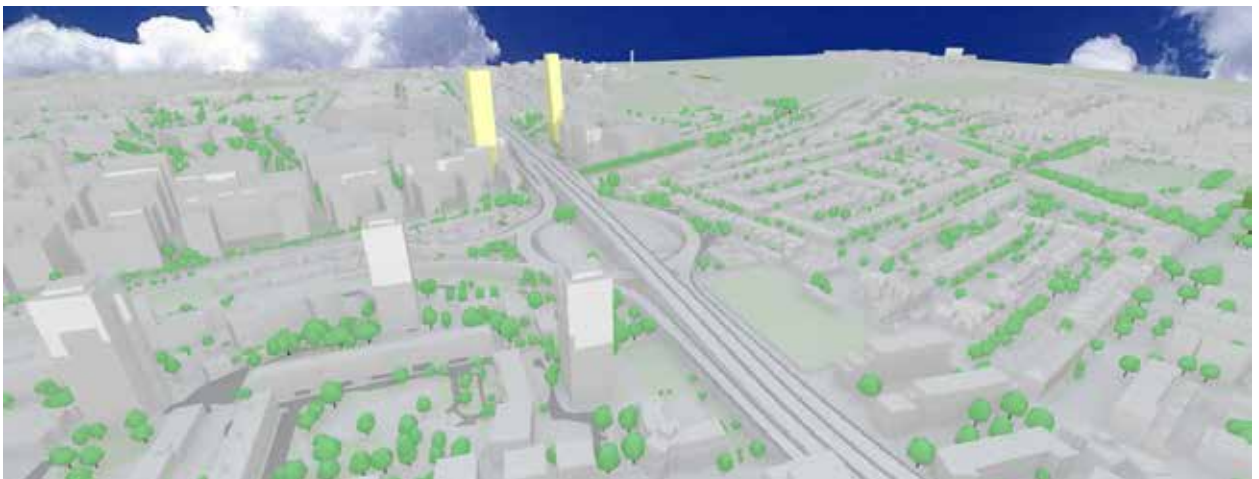


Figure 3.58 Tall buildings along the Westway - proposed

(Source 3D model:Z Mapping)



Figure 3.59 Key plan: Along the A40 / Westway

(Source 3D model:Z Mapping)

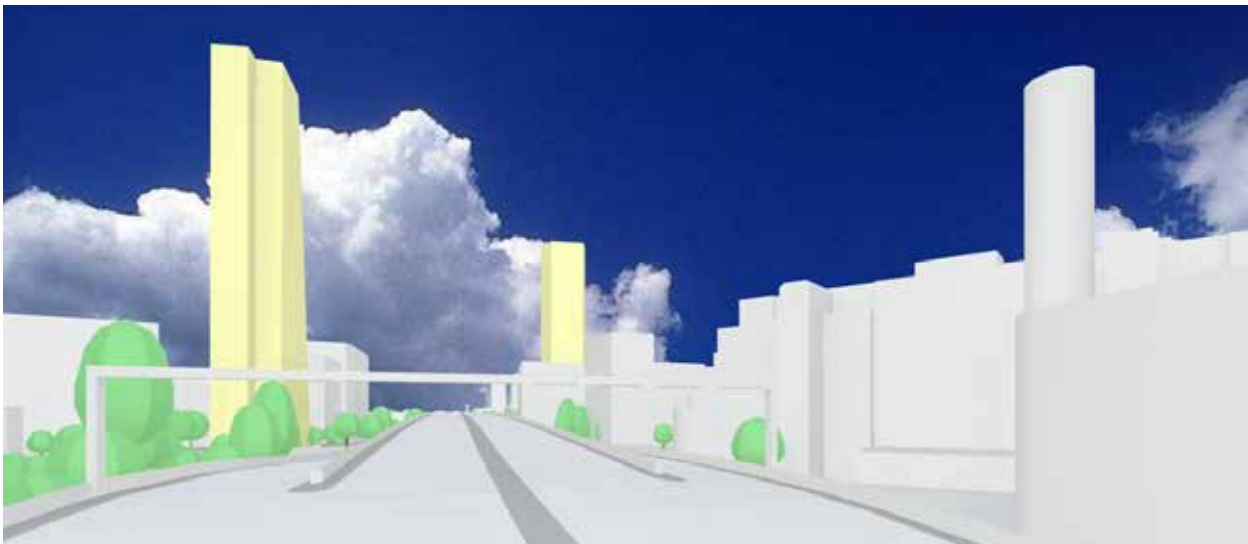


Figure 3.60: View 1, Looking east along the A40 / Westway

(Source 3D model:Z Mapping)

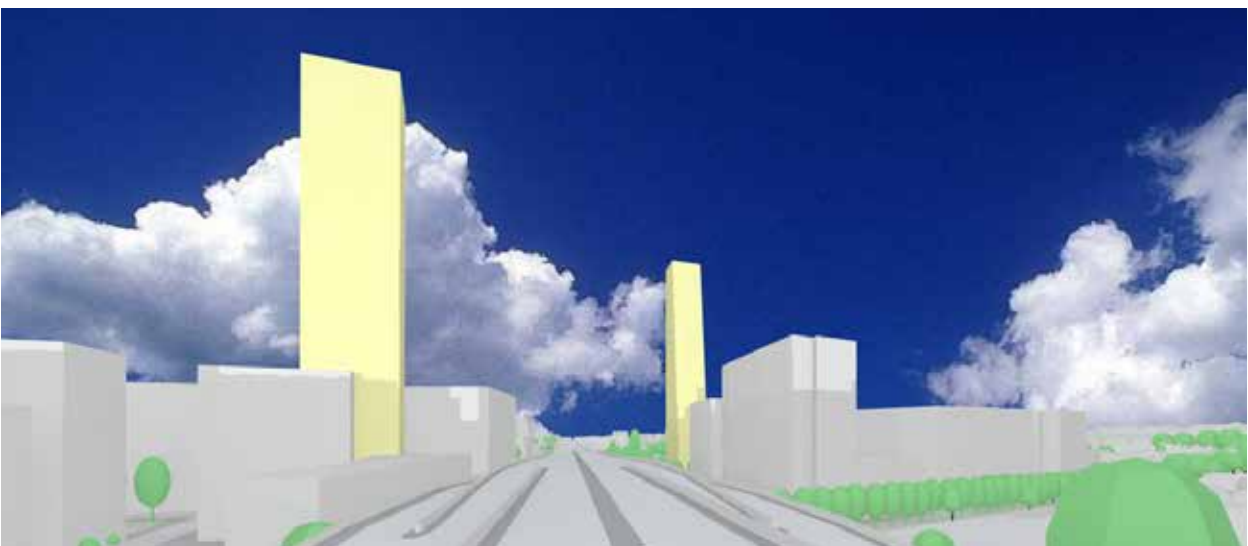


Figure 3.61: View 2, Looking west along the A40 / Westway

(Source 3D model:Z Mapping)

At White City Green

3.148 Two taller buildings of 11-20 storeys may be justified as part of an integrated design approach to highlight the area as a centre of activity. The taller buildings around the Green could provide a recognisable and identifiable address for new commercial and residential developments, frame views to the BBC from the east, complement the delivery of White City Green and nearby community and leisure facilities; and capitalise on the prime location fronting open space. When viewed from the Westway and the long cityscape views, the buildings would be read as part of the cluster of tall buildings (21-24 storeys) to the east of the West Cross Route.

3.149 The indicative masterplan illustrates one 11-15 storey building adjacent to the north-west



Figure 3.62 Taller building on the Westfield site

corner of White City Green to signal the location of the commercial hub and White City Station.

North of Westfield

3.150 There may be scope for a taller building (up to 20 storeys) to the north of the existing Westfield Centre at the existing crossing over the West Cross Route. This would place the building on an axis with the east tower of the BBC Television Centre to assist with orientation, highlighting the crossing point into RBKC.

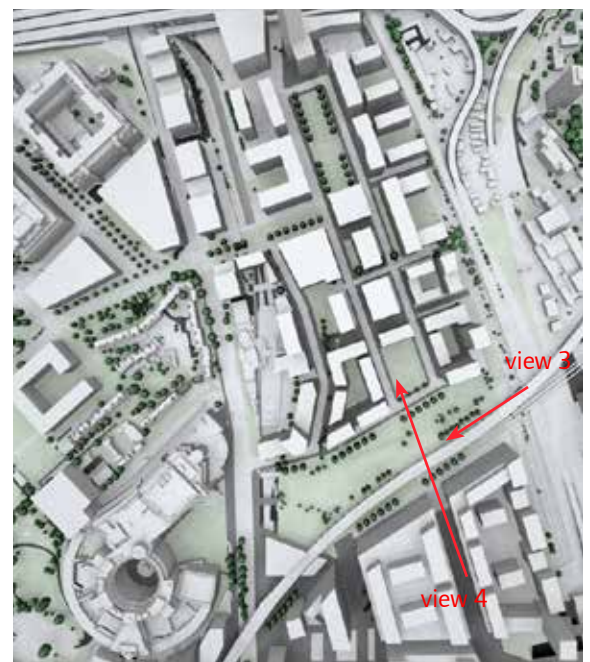


Figure 3.63: Key plan, White City East
(Source 3D model:Z Mapping)



Figure 3.64: View 3, Looking west from the Hammersmith & City Line Viaduct

(Source 3D model:Z Mapping)



Figure 3.65: View 4, Looking north from White City green, towards the Westway

(Source 3D model:Z Mapping)

BBC TV Centre

3.151 The setting of the listed BBC Television Centre would be enhanced, particularly in views from the east, by the demolition of the existing east tower. A new building that could be taller than the existing building, but crucially of a much higher quality and with a better relationship to Wood Lane, and the Westfield development would sit on the axis of views from the West Cross Route, providing an east-west visual connection across the entire site.



Figure 3.66: Aerial view of BBC TV Centre and the existing tower

(Source: Westfield)



Figure 3.67: Indicative masterplan - aerial view of BBC TV Centre

(Source 3D model: Z Mapping)

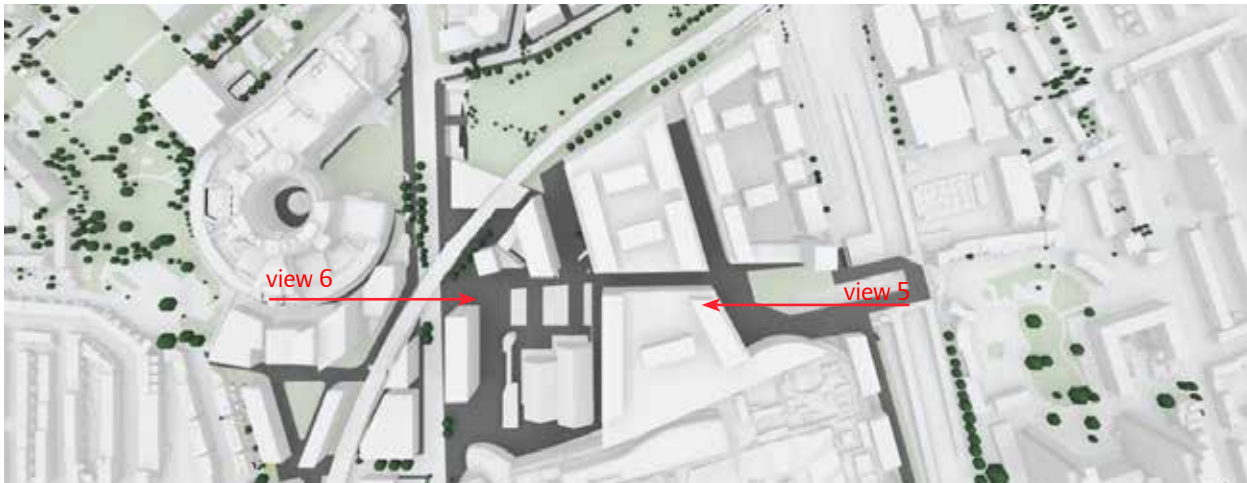


Figure 3.68: Key plan: Westfield High Street

(Source 3D model:Z Mapping)

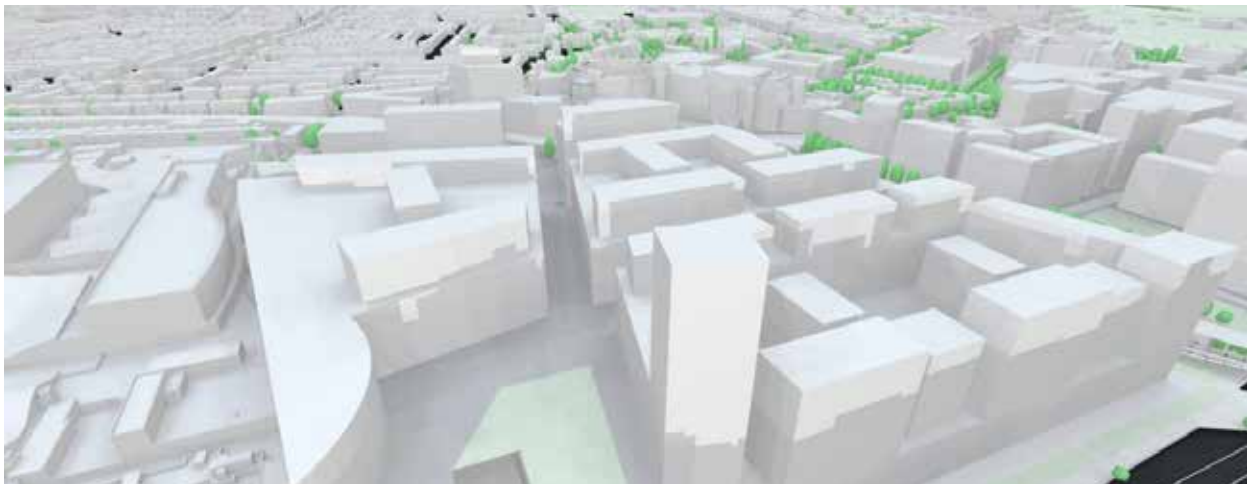


Figure 3.69: View 5, Looking west along Westfield High Street towards the east tower, from the West Cross Route junction

(Source 3D model:Z Mapping)

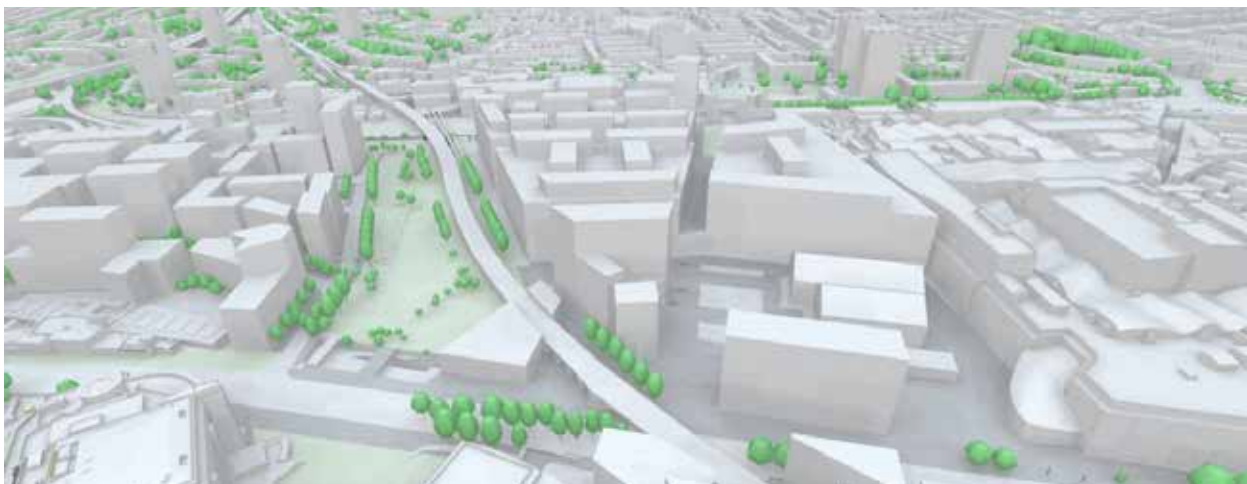


Figure 3.70: View 6, Looking east along Westfield High Street towards RBKC from Wood Lane and the BBC TV Centre

(Source 3D model:Z Mapping)

Views assessment of tall buildings

3.152 An assessment of the visual impact of taller buildings on views into and out of the OA has been undertaken. This includes views to and from RBKC and other local conservation areas. The context is a tightly-knit, densely developed inner London townscape

3.153 The Townscape Views Analysis (Appendix 1) was commissioned to supplement the work already undertaken regarding the visual impact of taller buildings within the OA on surrounding areas. It provides a baseline study of 21 views (see Figure 3.71) from outside the OA and considered the impact to the townscape as a result of the development of 5 key sites within WCOA (Imperial West, Dairy Crest, Westfield, BBC TV Centre and Marks and Spencer).

3.154 Buildings will be most visible from open spaces which afford panoramic views eg Wormwood Scrubs, Kensal Green Cemetery and Kensington Memorial Gardens. With only low level tree screening around their edges they afford views not only of the consented 35 storey tower on the Woodlands site and the approved 32 Storey tower on the Dairy Crest site but many of the anticipated mid-rise buildings too. However many will be viewed in the context of other existing tall buildings such as the Charecroft Estate Towers, Westfield Shopping Centre and BBC TV Centre.

3.155 In views from the northern part of the site from the east and west, e.g. from within the Oxford Gardens and the Wormholt and White City Conservation areas, two towers will be visible in views along the street grid whilst the mid-rise buildings will be largely obstructed or screened from view by existing buildings and tree cover.

3.156 From the south, the two tall towers are only prominent from the corner of Wood Lane and Uxbridge Road and they create new distant landmarks as a focus for the regeneration of the area, and provide a visual link to the town centre. In other views from the south such as Shepherds Bush Common, new buildings will be completely or partially screened from view by existing development and mature trees. In Royal Crescent and Holland Park, buildings are largely obscured from view by existing buildings and mature trees.

3.157 In views of the central and southern part of the site from the west e.g. Avondale Park and Ladbrooke Grove Conservation Areas, the towers are partially visible, being largely obscured by existing buildings and screened by mature trees.

3.158 A more detailed analysis with other views will be likely to be required with any particular proposal which comes forward as part of a planning application.



Map 3.71: Views from Townscape Views Analysis (TA1)

Progress to masterplan delivery 2011-2013

3.159 This previous indicative masterplan was presented to stakeholders and the public for the

first round of consultation in 2011. This plan has since been refined as a result of consultation comments and negotiations with developers and key stakeholders over emerging proposals.



Figure 3.72: Masterplan consulted on in 2011

(Source 3D model:Z Mapping)

3.160 The 2013 masterplan reflects progress towards the implementation of the original masterplan.

Implemented
Planning permission consented

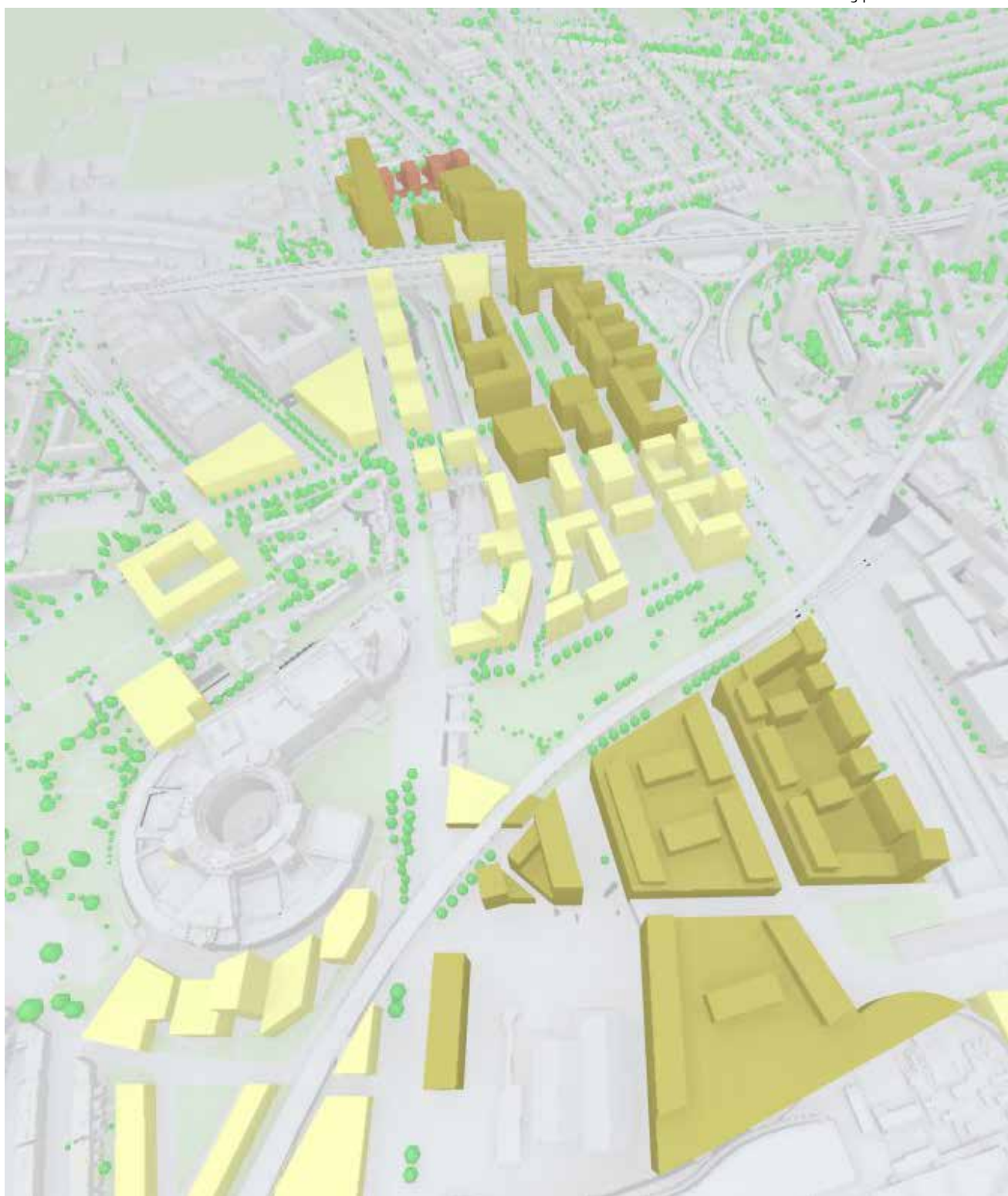


Figure 3.73: Current masterplan proposals 2013

(Source 3D model: Z Mapping)

